



**Prepared For:**

City of Lompoc  
Planning Division  
100 Civic Center Plaza  
P.O. Box 8001  
Lompoc, CA 93438

## Central Coast Business Park

### *Specific Plan*



# **Central Coast Business Park**

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### **SP 14-01**

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**October 2015**

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## 1.0 INTRODUCTION

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### 1.1 PURPOSE AND INTENT

The Central Coast Business Park Specific Plan (CCBPSP) provides a comprehensive plan and regulations to guide development within this CCBPSP area in the City of Lompoc. The General Plan land use designation for the CCBPSP area is Business Park (BP) and the zoning designation is Business Park (BP).

This CCBPSP establishes the regulations, programs, and procedures required for the systematic implementation of the General Plan goals and policies for this area of the City, as it requires a more comprehensive and intensive evaluation and planning effort due to its size, the need to master plan infrastructure, and the unique environmental setting and viewshed.

The Specific Plan facilitates development of the CCBPSP area as a master-planned business park that includes a variety of manufacturing, warehouse, hangar/storage, and office space.

This CCBPSP complies with the requirements of the California Government Code (Sections 65450 et seq.), which addresses the required contents of specific plans, and Chapter 17.080 of the Lompoc Municipal Code, which supplements the state requirements with optional subjects, minimum contents, and provides for fees and an additional environmental review procedure and is intended for adoption by resolution for General Plan Consistency and by ordinance for zoning consistency by the City.

### 1.2 LOCATION AND SETTING

**Exhibit 1, Regional and Site Location**, illustrates the location of the CCBPSP area within the City of Lompoc (City), and the County of Santa Barbara, California. Regional access is provided by Highway 1 and Highway 246, which link the City to Highway 101. From a local perspective, the CCBPSP area is located at 1401 West Central Avenue between V Street and Barton Avenue on the north side of Central Avenue. The CCBPSP area includes 40 gross acres and currently consists of three parcels (Assessor Parcel No. [APN] 93-450-014,-015, and -016).

**Exhibit 2, Aerial of Specific Plan Area**, illustrates the existing land uses on, and surrounding, the CCBPSP area. The CCBPSP area is currently used for the agricultural production of various row crops, such as green cabbage and cauliflower. A sand and gravel mine, animal services facility, and a solid waste operations yard are located to the west of the CCBPSP area across V Street. Immediately north of the CCBPSP area is the Lompoc Airport, with the south-side taxiway located approximately 150 feet north of the property line. To the east, along the northern portion of the CCBPSP area, is vacant land designated for airport/aviation uses by the General Plan. Along the southern portion of the CCBPSP area, across Barton Avenue, is the Pali Wine Company and vacant land designated for business park uses by the

General Plan. A residential neighborhood is located to the south across Central Avenue, approximately 75 feet from the CCBPSP area.

### **1.3 PURPOSE OF THE SPECIFIC PLAN**

The purpose of the Central Coast Business Park Specific Plan is to:

- Develop the Business Park Zoning District and appropriate development standards.
- Prepare design guidelines for architecture, storefront design, landscaping, and signs that enhance the CCBP image and encourage compatible manufacturing and warehouse uses.
- Develop an appropriate urban design and streetscape concept plan for the CCBP.
- Encourage the development of the CCBP as a center for manufacturing and warehouse uses.
- Ensure consistency with the Lompoc General Plan.

The CCBPSP has been developed to allow a mix of the manufacturing, warehouse, hangar/storage, and office uses currently permitted in the City of Lompoc's Business Park Zoning District.

The high quality contemporary architectural design concept will allow groupings of professional, administrative, and high-technology research and manufacturing uses, accompanied by limited commercial activities to support such uses. The sizes of the proposed parcels and roadway layout is planned to achieve orderly and logical circulation for the light industrial and office uses envisioned within the CCBPSP.

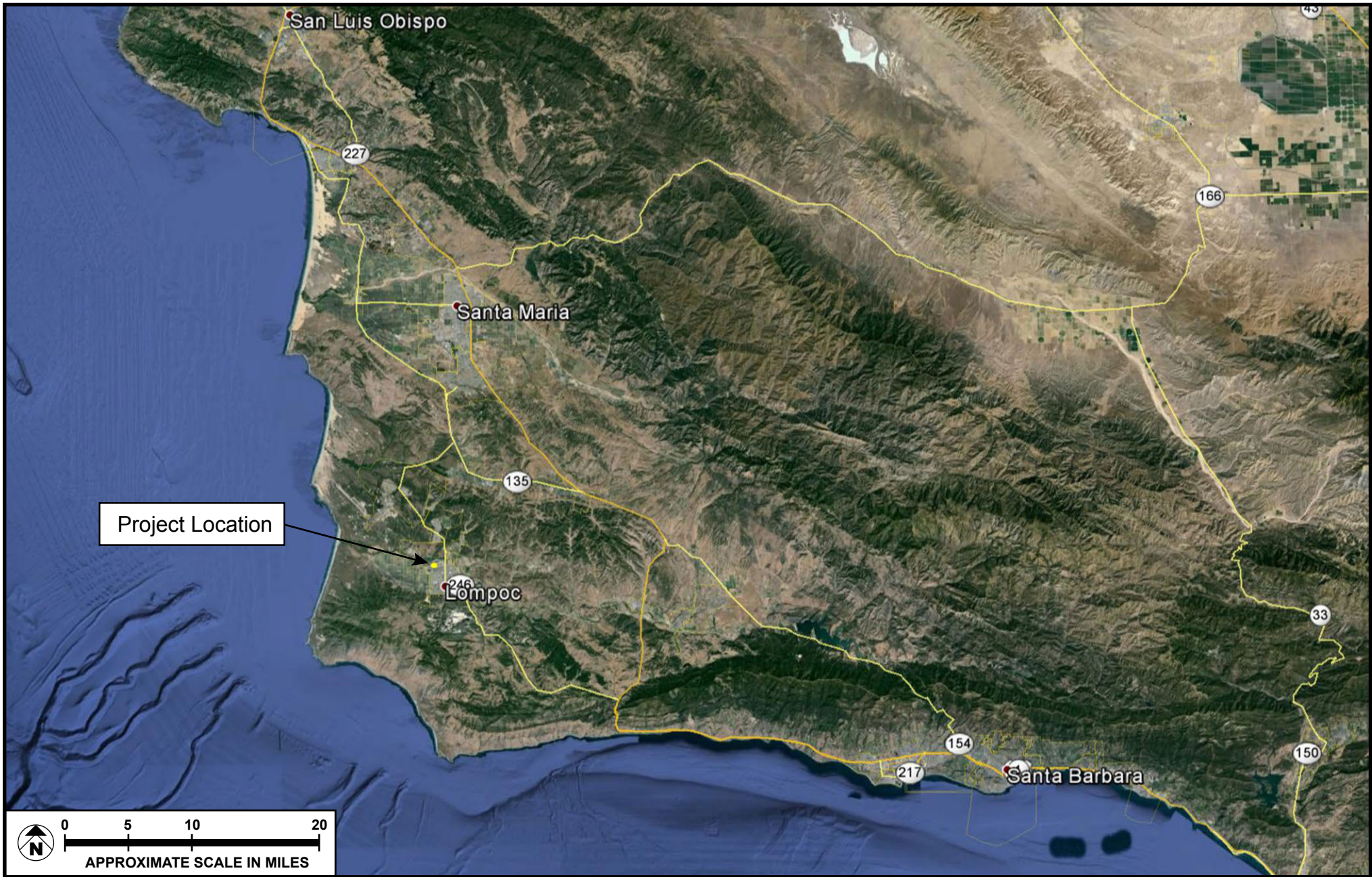


EXHIBIT 1



## 2.0 CONCEPTUAL LAND USE AND CIRCULATION PLANS

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### 2.1 CONCEPTUAL LAND USE PLAN

This CCBPSP allows a Business Park planned development containing a mixture of manufacturing, warehouse, hangar/storage space, and office uses. This section identifies permitted, conditionally permitted, and prohibited land uses within the CCBPSP area. Where a use is unlisted, the Economic Development Director, or designee, shall be responsible to make a determination of the most appropriate use classification. Unless the proposed use is determined to be similar to a listed use, it shall be prohibited.

**Exhibit 3, Conceptual Land Use Plan** illustrates the configuration of lots planned for the Business Park. Development, either permitted by right or requiring a Conditional Use Permit (CUP), will be in accordance with current codes and ordinances of the City of Lompoc, as identified in **Table 2.0-1, Permitted and Conditional Uses**.

The CCBPSP site area will be subdivided developed into 12 separate parcels, ranging in size between 2.4 acres and 3.5 acres.

**Table 2.0-1  
Permitted and Conditional Uses**

Land Use Category	Permit Requirement
Animal Hospitals, Kennels and Veterinary Clinics	CUP
Assembly (small scale)	CUP
Assembly (large scale)	CUP
Catering establishment and box lunch preparation	CUP
Commercial Service Businesses –10,000 square feet or smaller	P
Commercial Service Businesses – over 10,000 square feet	P
Day Care Center	CUP
Day Care Center, Employer-Sponsored Child	P
Distribution (low-intensity, e.g. local distribution)	P
Distribution (high intensity, e.g. regional distribution, freight terminal)	P
Distribution – wholesale alcohol	P
Equipment Rental	CUP
New Farm Vehicle Sales, storage, repair and auction	CUP
Food Processing (wholesale)	P
Furniture Sales, Rentals or Repairs	CUP
Health Clubs	CUP

Land Use Category	Permit Requirement
Laboratories	CUP
Manufacturing (small scale)	P
Manufacturing (large scale)	P
Motion Picture Studio	P
Greenhouse	P
Offices – As primary use or accessory to primary industrial use	P
Processing (small scale)	P
Processing (large scale)	P
Public Facilities for governmental purposes	P
Public storage facility/mini warehouse	P
Public Utility Yard	P
Recreational enterprises and facilities	CUP
Research and Development Businesses	P
Storage (large scale, e.g. construction equipment, building Materials, operating motor vehicles, even when in conjunction with another use on the same lot or parcel)	P
Storage – non-operating motor vehicles, even when in conjunction with another use on the same lot or parcel	CUP
Telecommunication Facilities	CUP
Trade School	CUP
Utility Distribution and Transmission Stations	P
Vehicle parts and equipment distribution	CUP
Vehicle sales – wholesale	CUP
Vehicle rental agency	CUP
Warehouse (other than distribution)	P
Wholesaling	P
Winery, tasting room	CUP
Churches	CUP

Legend: P = Permitted Use  
CUP = Conditional Use Permit

## 2.2 CONCEPTUAL CIRCULATION PLAN

The Conceptual Circulation Plan for the CCBPSP provides a framework and standards for road development to ensure a safe and adequate system of vehicular and pedestrian and bicycle circulation, as depicted in **Exhibit 4, Conceptual Circulation Plan**. In addition, the standards for parking are specified. This Conceptual Circulation Plan is intended to:

- Ensure the street layout and design considers geologic conditions, drainage patterns, and storm water flow.
- Ensure the street system integrates with developments in adjacent areas
- Ensure streets are classified and designed according to the land uses and traffic volumes which they will serve
- Plan a street system which considers the safety, convenience, and economy of construction in its design
- Provide roadway landscaping which will enhance the CCBPSP design.

### ***Vehicular Circulation Plan***

The Vehicular Circulation Plan is depicted in **Exhibit 4**. The circulation system consists of Central Avenue, Barton Avenue, V Street, Aviation Drive, and Avila Court. Primary north/south access to the CCBPSP is provided by Barton Avenue and V Street and east/west access is from Aviation Drive.

**Exhibit 5, Street Sections**, detail each street section, which includes the dedicated right-of-way and build-out. The Vehicular Circulation Plan is subject to modification, based on the final site plan.

The street sections proposed in this CCBPSP shall be constructed according to all City radius, crown, curb, and pavement specifications. A “no access easement” shall be designated along Central Avenue.

### ***Bicycle Circulation***

Class II Bikeways are routes that provide a right-of-way for bicycles and pedestrians within a roadway in which motor vehicle parking and cross-flows are permitted; and Class III Bikeways are routes which provide a right-of-way within the paved area of the roadway, designated by signs or markings on the pavement.

As shown in **Exhibit 4**, the CCBPSP shall incorporate additional Class II Bikeways on V Street and Central Avenue and a Class III Bikeway along both sides of Aviation Drive to allow bicyclists to access the site and along adjacent roadways.

The Class II and Class III Bikeways shall be constructed according to Caltrans standards.

### ***Pedestrian Circulation***

Sidewalks are integrated into the landscaped streetscape. The Conceptual Landscape Streetscape Plan contained in **Section 3.1.2** and **Exhibits 6, 7, and 8** detail streetscape designs.

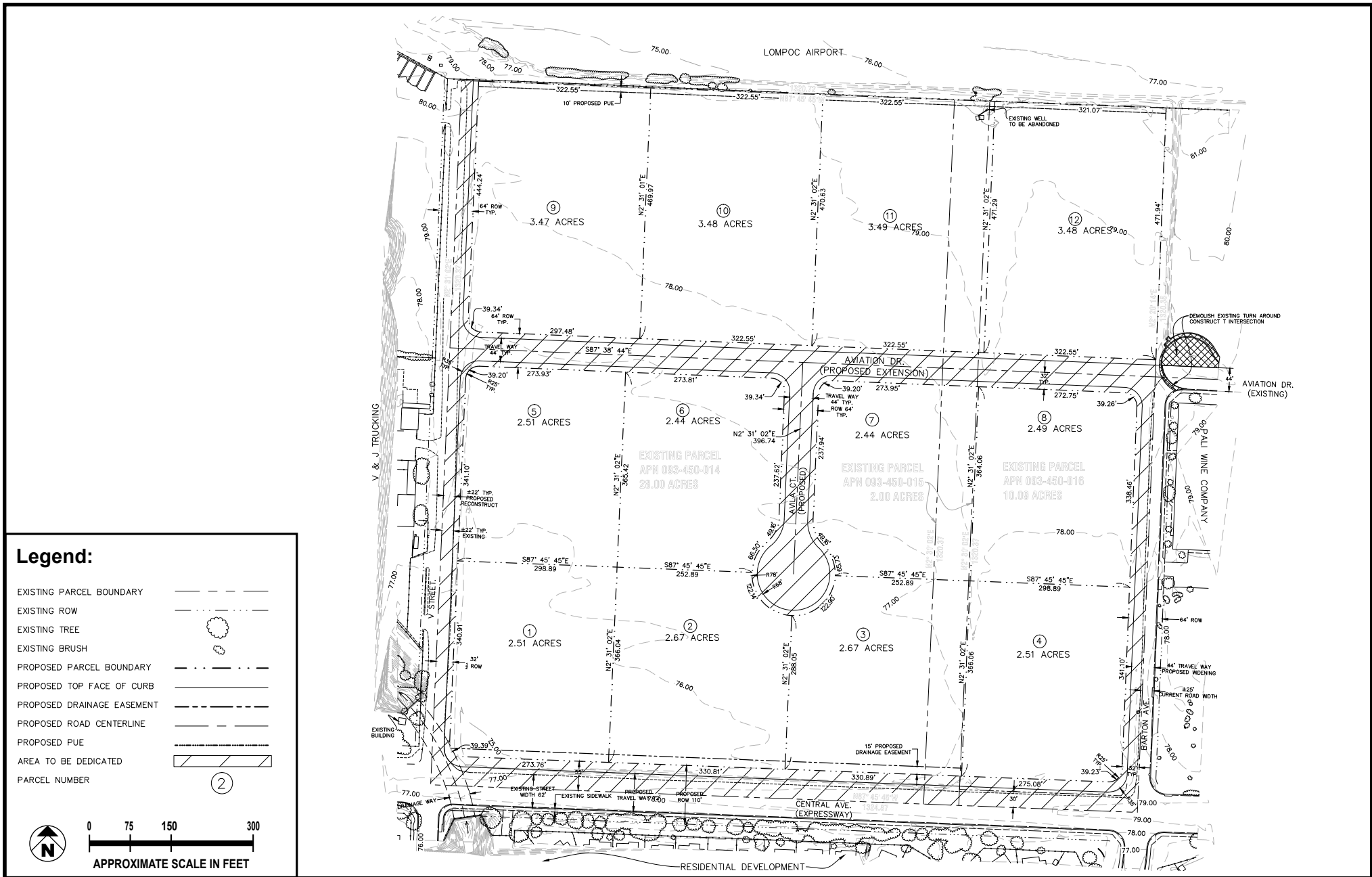
### ***Parking Regulations and Standards***

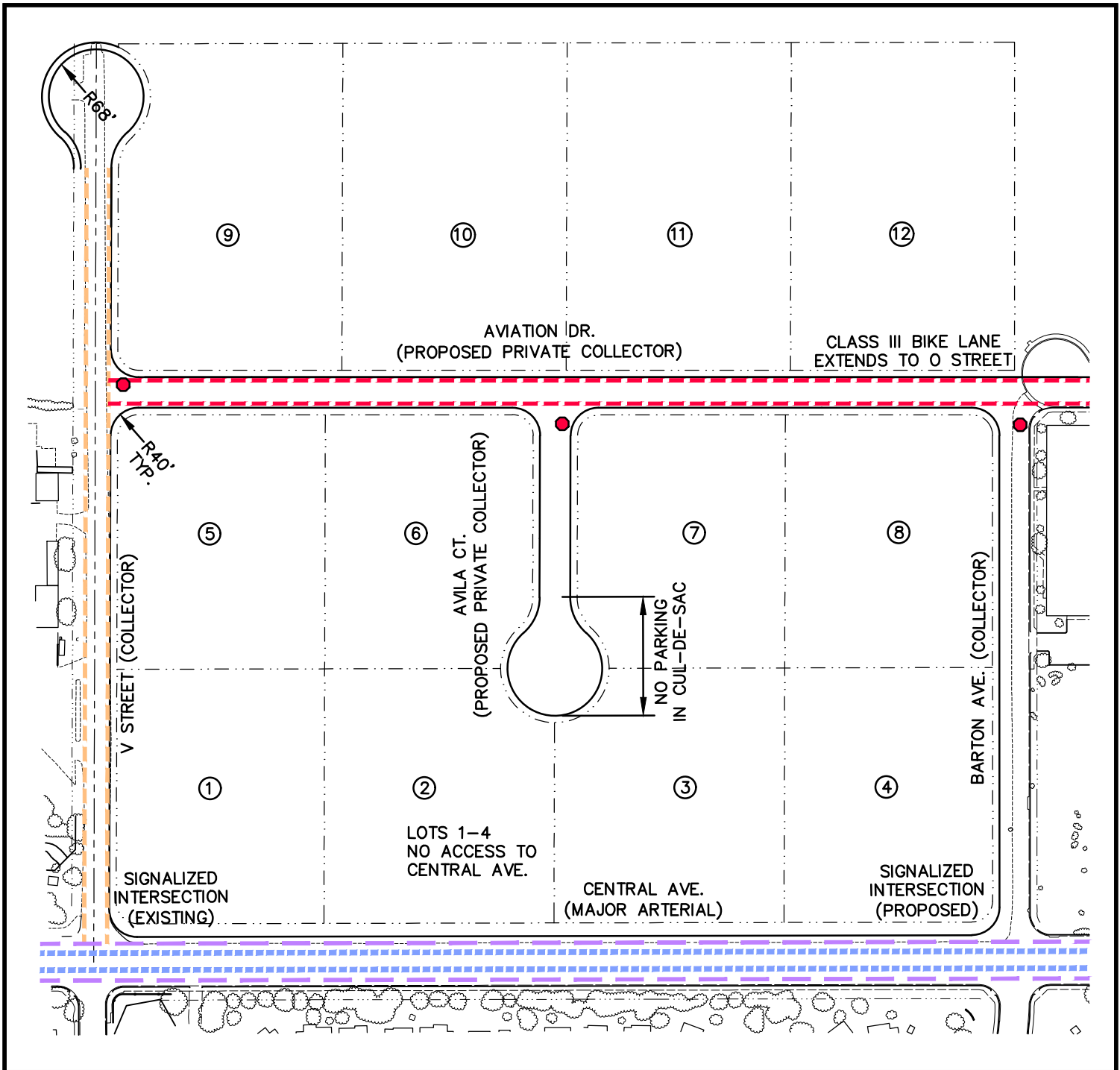
Parking regulations and standards ensure that the CCBPSP contains sufficient on-site parking and loading facilities for the uses proposed and that these on-site parking and loading areas enhance and preserve the appearance, character, and value of the CCBPSP.

All vehicle on-site parking and loading facilities, planned for and constructed within the CCBPSP, shall comply with the Lompoc Municipal Code Section 17.112.010, Parking Regulations.

### ***Lompoc Airport Access***

The CCBPSP shall allow airplanes direct access to the Lompoc Airport to the north. Only the northernmost parcels of the CCBPSP site shall be allowed this option (Parcel Nos. 9, 10, 11, and 12). Any development proposing this access is required to coordinate with the City regarding an Access Agreement and associated fee. The fee will be based on the loss of revenue to the Lompoc Airport for the use of leasable space for access. Airport access also requires Federal Aviation Administration approval.





STREET AND BIKE LANE DESIGNATION FROM CITY OF LOMPOC GENERAL PLAN CIRCULATION ELEMENT.






PROPOSED PRIVATE STREETS WILL REQUIRE UTILITY EASEMENTS TO INCLUDE UTILITIES, AND PEDESTRIAN AND BICYCLE ACCESS EASEMENTS.

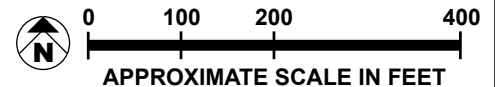
REQUIRED BIKE LANE SIGNS SHALL BE PLACED AS PART OF PHASE 1 IMPROVEMENTS.

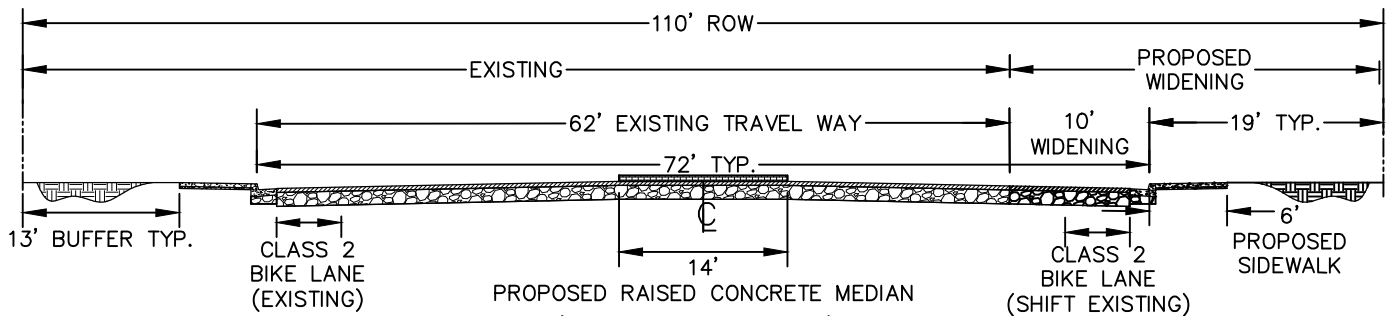
NO STRIPING IS REQUIRE FOR CLASS III BIKE LANE

EASTERLY HALF OF CUL-DE-SAC ON V STREET TO BE CONSTRUCTED AS PART OF IMPROVEMENTS.

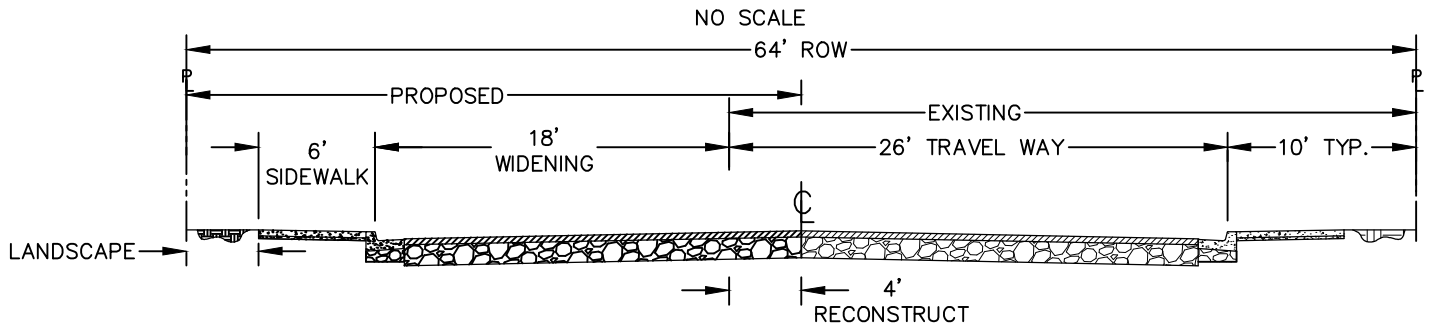
**Legend:**

-  TRUCK ROUTE
-  EXISTING CLASS II BIKE LANE
-  PLANNED CLASS II BIKE LANE
-  PLANNED CLASS III BIKE LANE
-  PLANNED STOP SIGN

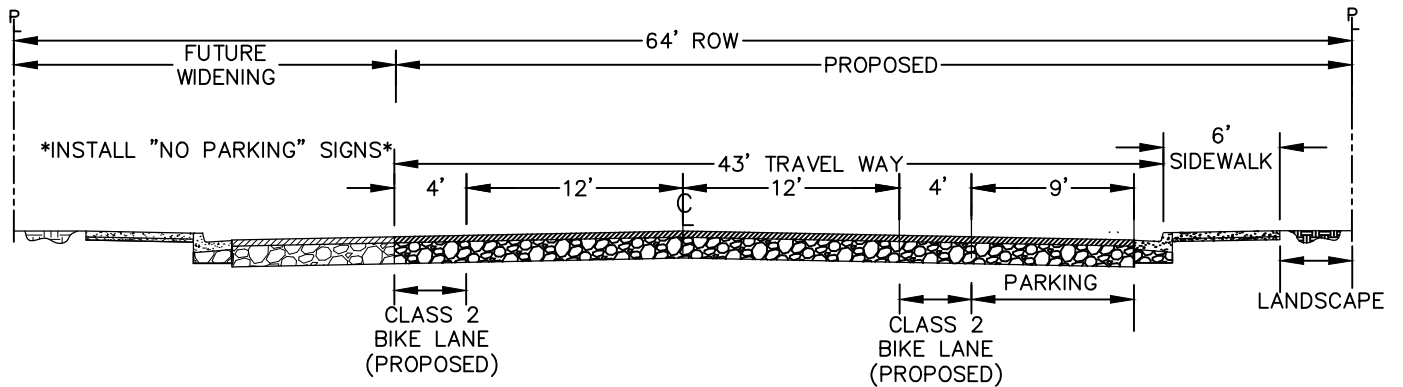




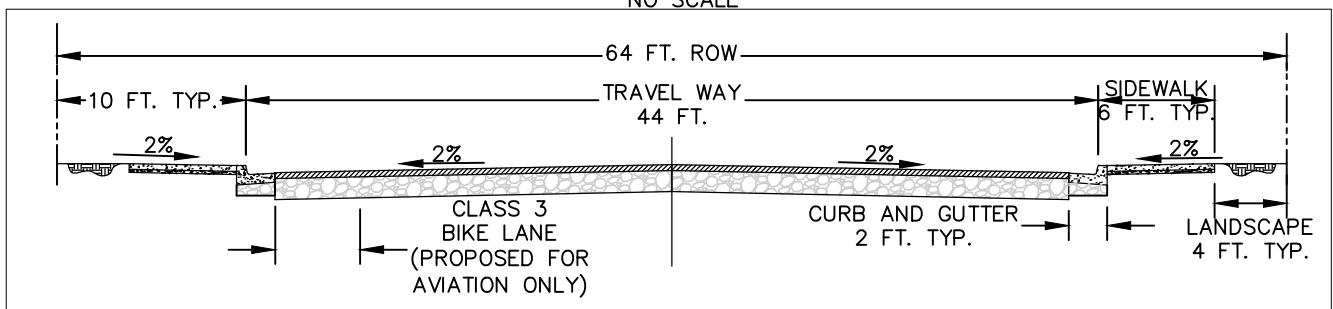
**PROPOSED CENTRAL AVE. (MAJOR ARTERIAL) WIDENING LOOKING WEST**



**PROPOSED BARTON AVE. (COLLECTOR) WIDENING LOOKING NORTH**



**PROPOSED V STREET (COLLECTOR) CONSTRUCT LOOKING NORTH**



**AVIATION DR. & AVILA CT.**

**TYPICAL PRIVATE COLLECTOR STREET LOOKING EAST OR NORTH AS APPROPRIATE**

NO SCALE

## 3.0 DEVELOPMENT STANDARDS AND DESIGN GUIDELINES

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This section contains the regulations, standards, and guidelines by which development must abide, as indicated by the use of the word “shall.” These regulations are mandatory and cover general development standards, nonconformities, lighting standards, sign programs, maintenance standards, and other standards for accessory structures. Provisions within these standards may also use the word “should,” in which case the standard is encouraged but not mandatory. Any standards not specifically covered by this CCBPSP are subject to the regulations of the City of Lompoc Zoning Ordinance.

### 3.1 DEVELOPMENT STANDARDS

Development standards control the building envelopes for the proposed manufacturing, warehouse, hangar/storage, and office space uses. These regulations have been designed to provide flexibility in site design while ensuring a consistent and coordinated built environment.

#### 3.1.1 Zoning Development Standards

The following requirements apply to development within the CCBPSP area:

Minimum Lot Area	2 acres
Minimum Lot Width	Minimum 150 foot frontage
Minimum Lot Depth	No minimum
Maximum Building Height	35 feet/18 feet along the northern boundary adjacent to the Lompoc Airport. Maximum building heights shall be required to meet Part 77 of the Federal Aviation Administration (FAA) regulations (14 CFR Part 77).
Maximum Floor Area Ratio (FAR)	0.75
Minimum Yard Setbacks	
Front yard	10 feet for building and parking
Side yard	5 feet for building and parking
Rear yard	5 feet for building and parking
	Dedication of an aviation easement along the northern perimeter south of the Lompoc Airport may be required.
Maximum Fence/Wall Height	8 feet; higher walls may be allowed subject to CUP
Fence/Wall Materials	Wrought iron, solid masonry; chain link. Barbed/razor wire may be allowed when minimized from public view, and subject to CUP from Planning Commission
Parking Requirements	In accordance with Section 17.112.010 of the Lompoc Municipal Code

Outdoor Storage	Screened by walls and landscaping. Cannot comprise more than 50 percent of the primary use site area of the rear portion of the parcel when adequately screened. No material stored higher than the height of the required wall or fence, within 10 feet of the required wall or fence.
Nuisance criteria	In accordance with Section 17.064.050, Property Development Standards of the Lompoc Municipal Code

### 3.1.2 Landscape Standards

The Conceptual Landscape Plan, as shown in **Exhibit 6, Conceptual Landscape Plan**, was developed to meet the landscape standards of the City, and consists of plant and man-made exterior elements that give form to the exterior spaces. They are formed and accentuated by streets and building setbacks, the arrangement of project entrances, parking lots, buildings, and services areas, variety and placement of identification signage, site lighting, walkways, and a variety of drought tolerant plant materials that will unify the CCBPSP area.

The Landscape Plan is conceptual in nature and shall be further developed through the preliminary design development phase of each parcel, after the approval of the CCBPSP.

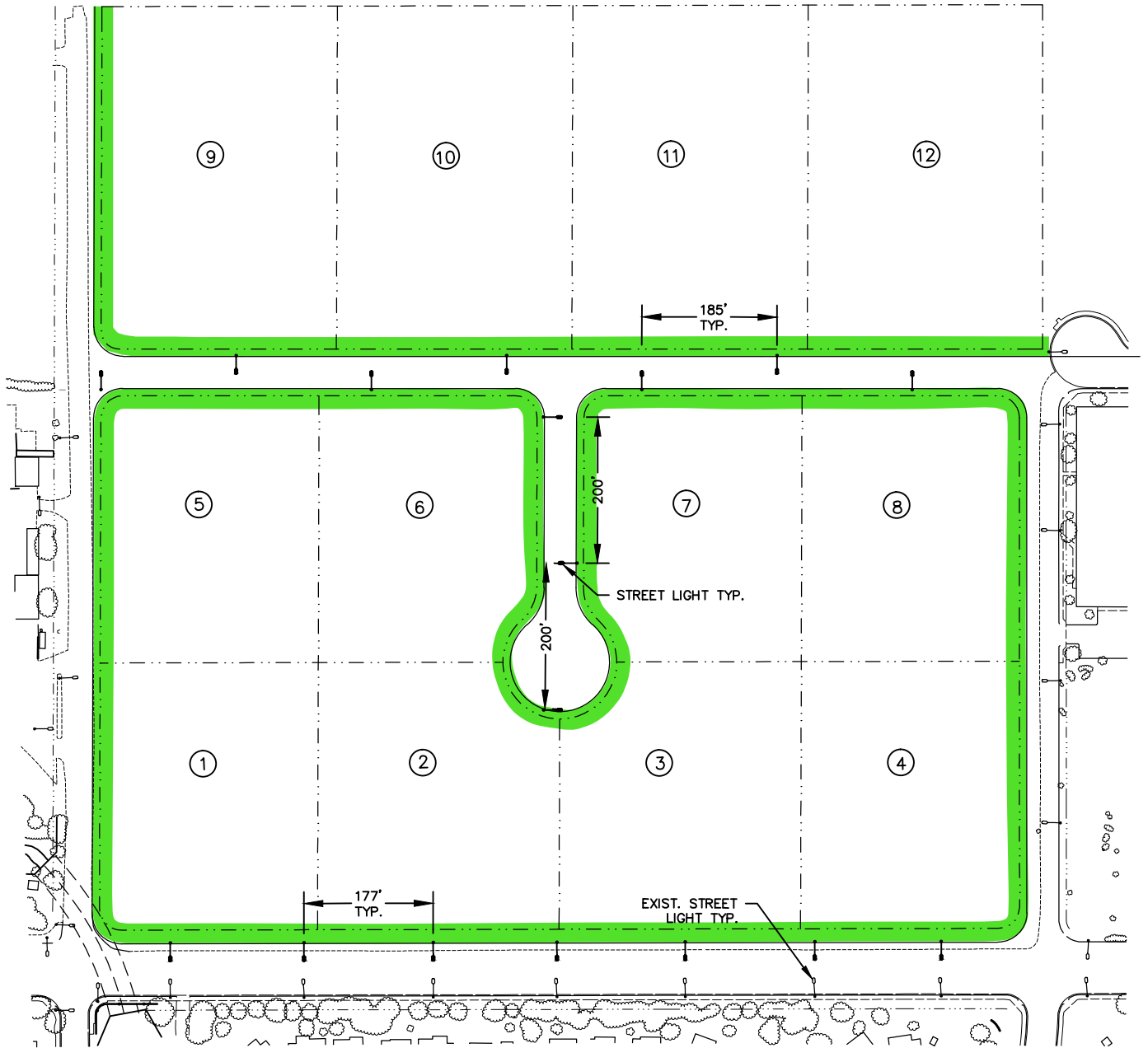
Landscape elements within the street frontage zone include sidewalks, irrigation, street lighting, project entry signage (where applicable), and landscaping. Central Avenue, Barton Avenue, and V Street shall incorporate a formal pattern of primary street trees with sidewalks. Streetscapes shall be installed as part of the improvement for each street. All street trees will be private and installed outside the public right-of-way.

At designated project entry locations, the entry monument sign(s) shall be placed (a minimum of five (5) feet) outside the public right-of-way.

Street landscaping and trees along internal private streets shall be maintained by the Property Owners Association (POA) or Assessment District. However, a maintenance agreement with the City will be required for landscaping along Central Avenue, Barton Avenue, and V Street. The POA or Assessment District will be responsible for maintaining the landscaping and the shared storm drain infrastructure. A maintenance agreement to this effect shall be recorded with the deed for each lot.

#### ***Parcel Entry and Interior Landscaping***

Entries into individual parcels shall include drought tolerant landscape components to enhance individual parcel identity. Each parcel developer shall be responsible for landscape and irrigation of parcel entry and interior landscaping, at the time of property development.



**LEGEND**

 Landscaped Areas



Parcel entries shall include a low, concrete wall with identifying monument signage and continuous screening (minimum 36 inches at time of planting) to screen cars from adjacent roadways.

Interior landscaping on individual parcels includes all exterior elements within an individual parcel and excludes parking lots, building perimeters, and bio-swales. Interior landscaping should meet the needs of each individual tenant and have a greater degree of design flexibility, while still meeting the drought tolerant overall concept. Design elements within these areas shall include landscape amenities, landscape diversity, site lighting, regulatory and directional signage, service area screening and side/rear property line treatment.

Parking lot landscaping requirements, for the parcel's interior zones, shall be as specified by the Lompoc Municipal Code. Landscape planters at the ends of each parking row shall conform to parking area landscape requirements outlined in the City parking regulations and standards for parking lot design.

The Side and Rear Yard areas of the parcel's interior zone shall have a maximum slope of 2:1 with a 1-foot minimum flat transition area provided at the top and bottom of all slopes within this zone.

All walls and fencing shall be screened in accordance with City landscape standards, including refuse collection sites that are located on site.

### ***Landscape Irrigation***

All planted landscape areas within the CCBPSP area shall have irrigation systems that are fully automatic and employ the latest low flow water conservation design criteria. There shall be no overspray of irrigation onto walkways, common area hardscape areas, or on architectural walls, allowed.

### ***Landscape Maintenance***

The individual parcel owners shall assume responsibility for maintenance of all landscaping and irrigation systems that are located behind the street right-of-ways on all streets, including the private internal streets off Aviation Drive and Central Avenue.

### ***Landscape Guidelines***

The Landscape Concept was developed to meet the landscape standards of the City of Lompoc. The plan shows the conceptual location of the plant materials including street trees, accent trees, perimeter screening trees, and shrub plantings.

All landscaping shall conform, in accordance with Lompoc Municipal Code 15.52 and 13.04.070.

### 3.1.3 Lighting Standards

Outdoor lighting shall comply with Title 24, Part 6 of the California Code of Regulations, Energy Efficiency Standards for Nonresidential Buildings. **Exhibit 7, Conceptual Lighting Plan** shows the locations of the on-street light poles.

All lighting shall conform to the standards set forth in the Lompoc Municipal Code.

### 3.1.4 Sign Program

All signs shall comply with the CCBPSP sign requirements and the City of Lompoc Zoning Ordinance. In those instances where the CCBPSP and the zoning ordinance conflict, the CCBPSP sign program shall govern.

Signage is permitted to identify addresses, provide direction, and advertise businesses. Along with communicating information, signage should add to the character of the community and reinforce a sense of place. The sign program identifies the hierarchy of permitted signs with a common theme, and specifies the allowed signage location, size, style, and number of permitted signs.

All signs within the CCBPSP shall be maintained in an as-new condition. The property manager shall make periodic inspections of all signs on site. Any deficiencies shall be immediately corrected by the person(s) or business(es) responsible for the maintenance of said sign, or signs.

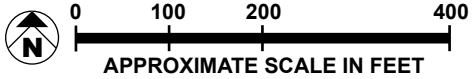
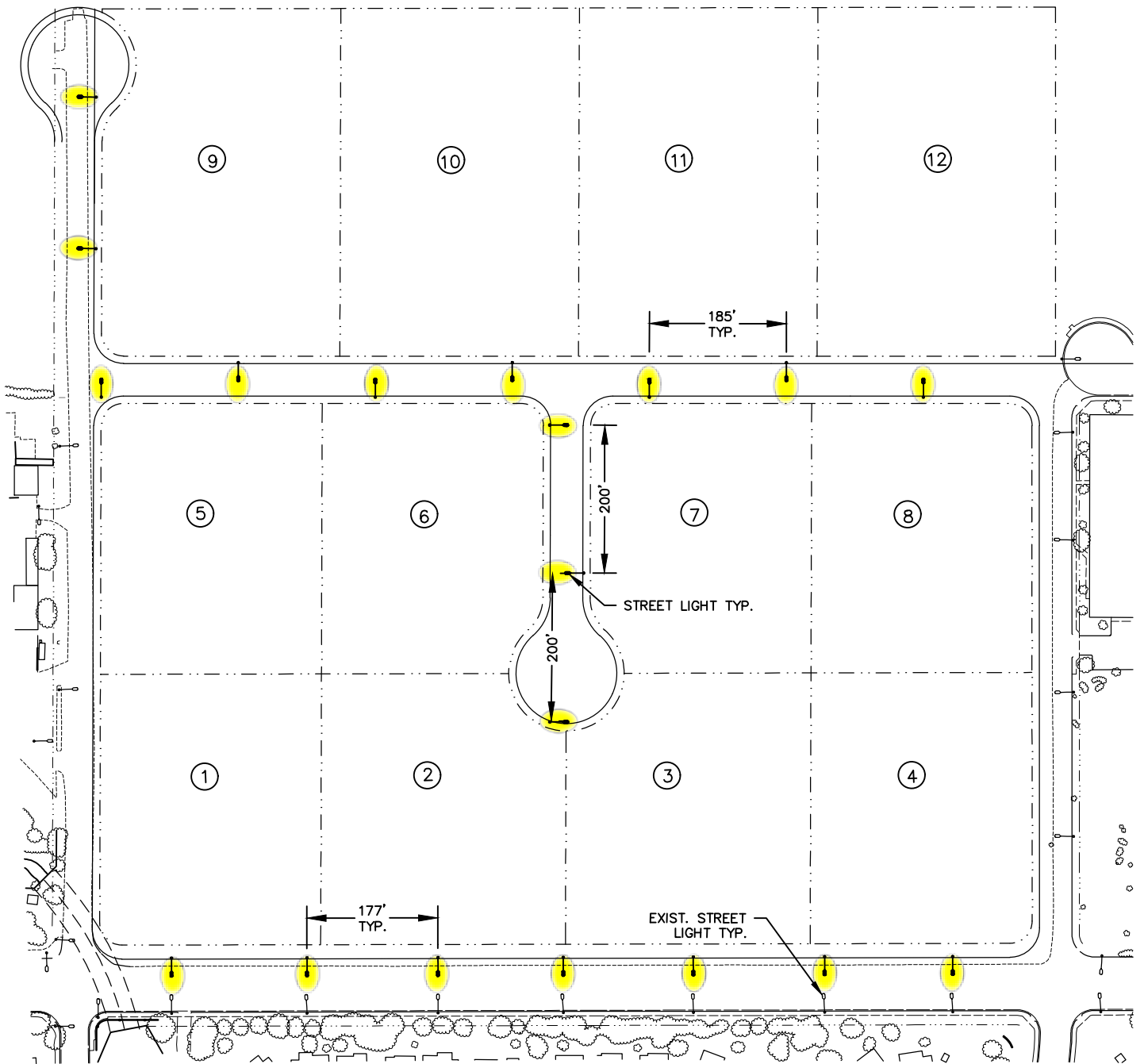
A sign application, including plans and drawings, shall be submitted by the individual building owner to the City Planning Division to assure conformance with the approved sign program and building permits to the Building Division.

No person shall erect, construct, enlarge, alter, move, improve, remove, convert or equip any sign or sign structure, or cause or permit the same to be done contrary to or in violation of the provisions of this Sign Program or City permit requirements. All such nonconforming or unapproved sign(s) must be brought into conformance at the expense of the persons responsible for the installation or alternation of said sign(s).

The purpose of these criteria is to establish standards and specifications to assure that all signage located within the CCBPSP will be consistent and enhance the identity of all businesses.

**LEGEND:**

- EXISTING LIGHT POLE
- PROPOSED LIGHT POLE



## **General Standards**

Signs are grouped into the following categories: Tenant/Multi-Tenant Identity Monument Signs, Wall Signs, Window Signs, and Site Identity Monument Signs. The size, location, and design of each sign shall not exceed the specific maximum limitations, and shall meet the minimum requirements, of the CCBPSP and Chapter 17.108 of the Lompoc Municipal Code.

Sign area means the sum of the area enclosed within a geometric form, or forms, drawn around all writing, representations, logos, emblems or designs on all surfaces (front and back) of the sign.

All installation hardware shall be stainless steel to prevent rust staining of building surface.

Monument signs shall be formed from concrete. Optional flood illumination of monument signs should be accomplished with ground mounted fixtures. Such fixtures shall be controlled to ensure that no light shall interfere with drivers, pedestrians or adjacent properties.

Wall signs shall be internally illuminated or non-illuminated and consist of aluminum or sheet metal pin-mounted channel lettering. Halo backlighting is allowed.

## **Submittals and Approvals**

1. Prior to sign fabrication:

- Where the parcel is leased from the property owner: Tenant or his sign contractor, shall submit for Landlord approval three (3) sets of complete and fully-dimensioned and detailed shop drawings. In addition, these submissions shall include elevations, color and material boards, and cross sectional diagrams. All Tenant submittals shall be reviewed by Landlord and/or its agent for conformance with the provisions of the City approved signage program.
- Following Landlord's approval of proposed signage, Tenant or his agent shall submit to the City, sign plans signed by Landlord, and applications for all permits for sign approval fabrication and installation by sign contractor.
- Where the parcel is independently owned: Owner, or his sign contractor, shall submit to the City for approval three (3) sets of complete and fully-dimensioned and detailed sign plans and applications for sign approval fabrication and installation. In addition, these submissions shall include elevations, color and material boards, and cross sectional diagrams. All submittals shall be reviewed for conformance with the provisions of the City approved signage program.

2. Fabrication and installation of all signs shall be performed in accordance with the standards and specifications outlined in these criteria and in the final approved plans and design drawings. Any

work deemed unacceptable shall be rejected and shall be corrected or modified at Tenant's or Owner's expense as required to meet the standards.

## **Monument Signs**

**1. Site Identity Monument Signs:** Identifies the CCBPSP entry points and shall be located at the main entrances to the CCBPSP.

- One at the corner of Barton and Central Avenue
- One at the corner of V Street and Central Avenue
- Set back a minimum of five (5) feet from the public right-of-way
- Constructed entirely of concrete (natural, colored, or painted), sacked, and patched with smooth painted finish and sandblasted finish
- Located at least one (1) foot from any utility easement
- Located in a minimum 70 square foot landscape area
- Optional exterior illumination directed to the sign face
- All future maintenance is responsibility of POA
- Maximum of eight (8) feet in height
- Maximum of 10 feet in length
- Lettering shall be Century Gothic, all upper case letters, as shown in **Exhibit 8, Site Identity Monument**

**2. Individual Parcel Monument Sign:** Identifies a building occupied by a single tenant or multiple tenants.

- One per parcel
- Set back a minimum of five (5) feet from the public right-of-way
- Constructed entirely of concrete (natural, colored, or painted), sacked, and patched with smooth painted finish and sandblasted finish
- Located at least one (1) foot from utilities
- Located in a minimum 70 square foot landscape area

- Optional exterior illumination directed to the sign face
- Maximum of eight (8) feet in height
- Maximum of 10 feet in length
- Lettering shall be Century Gothic with first letter in uppercase and remaining letters in lowercase (unless a specific logo is required), as shown in **Exhibit 9, Individual Tenant Identity Monument**

### ***On-Building Signs***

1. Each building may have one (1) on-building sign. Tenants with more than one (1) street frontage may have two (2) on-building signs.
2. A building housing one (1) business may have a sign area 48 inches in height and 72 inches in length.
3. A building housing more than one (1) business may have a directory type sign with a sign area of 48-inches in height and 72-inches in length, individual signs shall be a maximum of 24-inches in height within the approved sign area.
  - Landlord, at Landlord's sole discretion, will determine which tenants will have signage on a directory sign.
4. Every building shall have address numbers visible to public safety personnel from the public right-of-way in Century Gothic and be a minimum of 10 inches in height.
5. No wall signs shall have any visible means of attachment to the building. Both internally illuminated and non-illuminated signs shall be fabricated from aluminum or sheet metal. Aluminum and sheet metal shall be finished in natural or building accent color. Both internally illuminated and non-illuminated letterforms and logos shall have the returns (sides) finished in aluminum or sheet metal painted to match the face letter color.

### ***On Building Addressing***

Returns for all signs shall be finished in aluminum or sheet metal painted to match the face letter color.

All sign circuits are to be provided to sign location by Landlord.

All local, State, and national codes are to be strictly complied with. Fabrication and installation to be Underwriter Laboratory (UL) approved with the required markings.

Signs. Shall be continuously maintained, without dark spots, light leaks, or manufacturing defects. Visible seams in acrylic faces, un-plated steel and black iron materials are prohibited. All metal must be

rust inhibited or aluminum with the appropriate finish. All conduits, crossovers, transformers, wiring and manufacturing labels shall be concealed.

All signs are to be laid out so as to be proportionate to the area in which it is placed, as well as comply with the square footage limitations. Signs shall also be centered at the appropriate location on the building elevation, so it will be balanced in relation to the building as a whole (not necessarily centered on the tenant space). The appropriate location to be approved by Landlord.

Landlord reserves the right to reject any fabrication or installation that is below standard.

All building penetrations are to be sealed and completely water proof.

These signs are individual numerals and fabricated from aluminum or sheet metal. Aluminum and sheet metal shall be finished in natural or building accent color. Returns shall be finished in letter face color or finish. Numerals shall be in the Project typestyle of Century Gothic and shall be 10 inches minimum in height, as shown in **Exhibit 10, Typical Building Address**. The tenant building identification shall be completed by the parcel tenant.

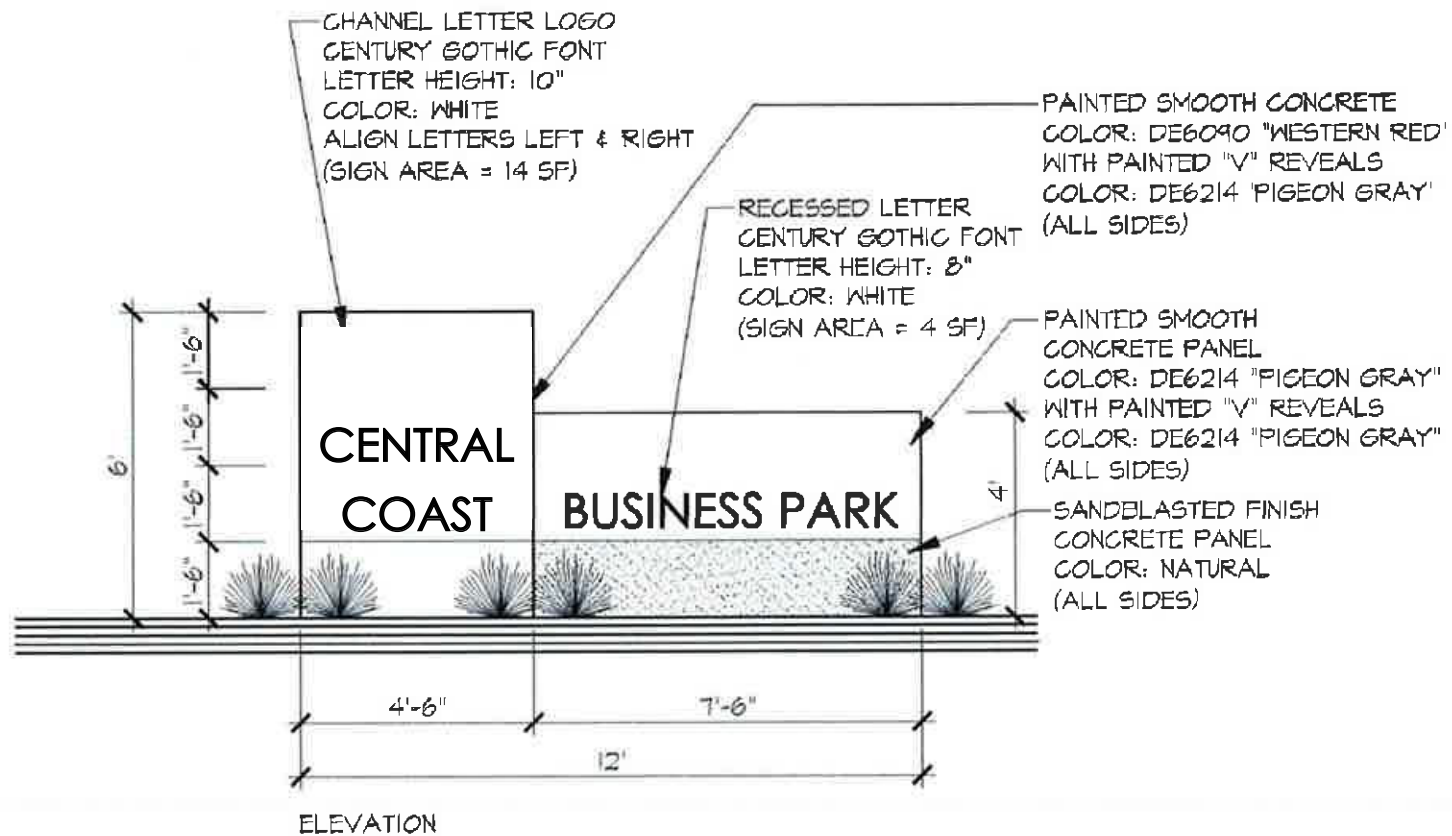
### ***Prohibited Signs***

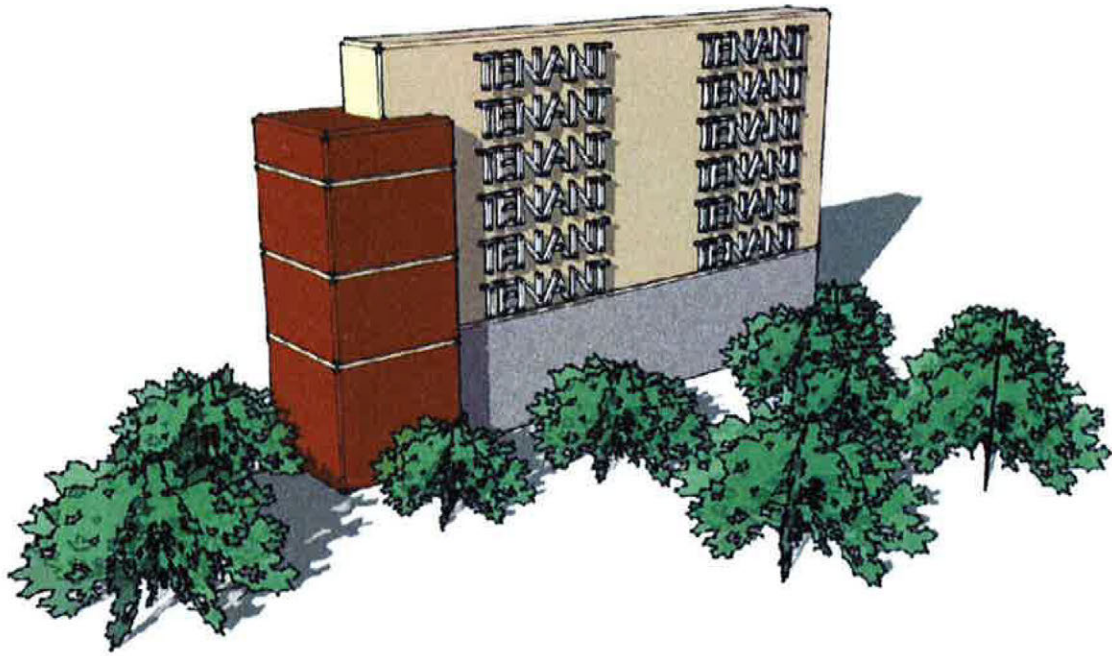
Temporary wall signs, pennants, flags, over the roof signs, inflatable displays, exposed neon, secondary signage, or sandwich boards as listed in Section 17.108.080 of the City Sign Regulations shall be prohibited.

Temporary banners advertising specials or sales are allowed for a maximum of 30 days in accordance with Section 17.108.070 of the City code. No more than one such sign shall be displayed on a legal parcel or lot; unless such parcel on which the individual business is located has two (2) or more street frontages, each of which has 200 feet or more, an additional sign shall be allowed for each such frontage.

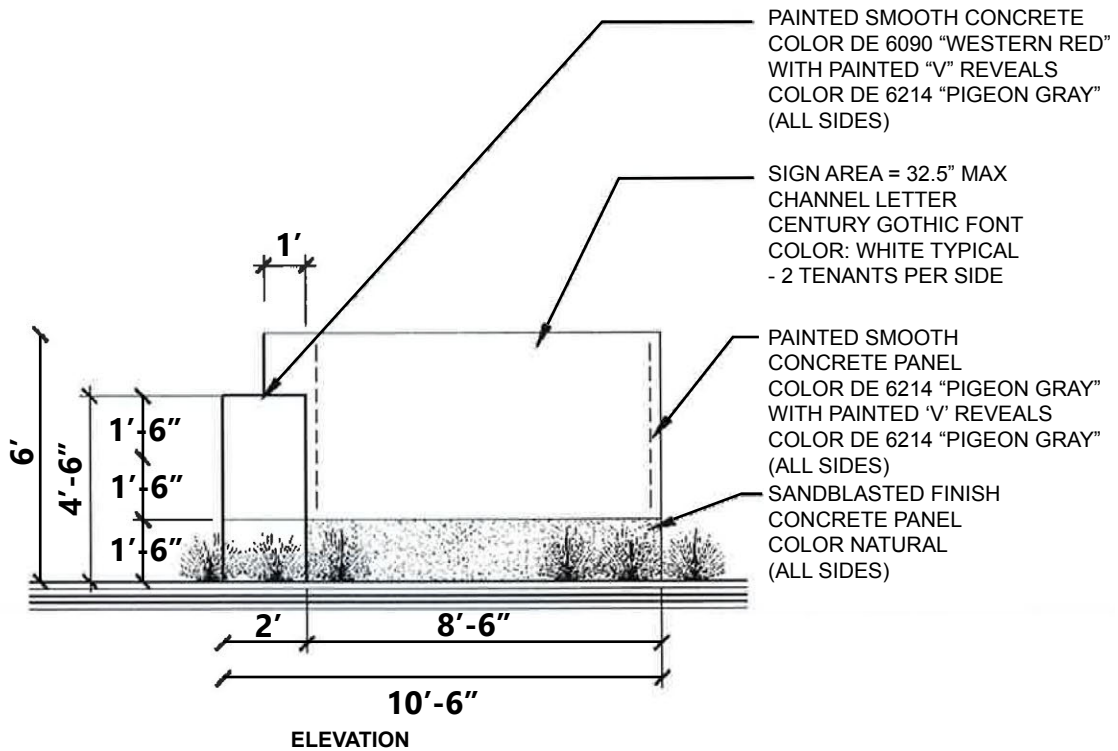


PLAN





PLAN





## 3.2 DESIGN GUIDELINES

Design Guidelines direct the style and aesthetic character of individual parcel developments and ensure a consistent use of building materials, landscaping, colors, and other design features. The Design Guidelines ensure that the CCBPSP area should have a clear identity and sense of place, and provide a harmonious and pleasing environment for all contemplated uses and activities. The guidelines will guide the development process including qualitative aspects of how buildings, landscaping, and permitted operations by tenants and owners will develop.

The guidelines include site design, building development, architectural, landscaping, and lighting guidelines. Each of the elements is detailed and coordinated with the other elements. The individual elements are discussed below.

### 3.2.1 Architectural Guidelines

Buildings with a high-quality agriculture-type architectural design theme within the Lompoc Valley (Architectural Area A) and industrial building architectural design theme (Architectural Area B) are encouraged, as depicted in **Exhibit 11, Architectural Themes**, and **Exhibit 12, Architectural Theme Areas**. The agricultural theme will create a regular pattern of elements interwoven with occasional meandering features such as pathways. Structures incorporated into this theme will have the character of rural, agricultural related materials and forms. Ramadas will have the appearance of a farm lean-to, barn, or outbuilding. Additionally, they will feature warm shades of yellows, greens, browns and gray accents that represent natural earth tones and colors. It is intended that buildings south of Aviation Drive will be designed using forms consistent with this theme. The design theme shall be consistent on all building elevations.

The industrial building architectural design theme is typified by the use of modular metallic elements that, anchored and suspended on the slab, covers the primary structure similar to buildings currently constructed within the Lompoc Airport. Additional building details shall include overhangs, pop-outs, and varied rooflines. Building materials and features include embossed sheet-metal panels and cast iron structures as well as tin ceilings, and fences. Colors should not be bright, reflective, metallic, or otherwise visually out of character with the natural setting. It is intended that buildings north of Aviation Drive will be designed using forms consistent with this theme.

The following architectural design features are recommended:

### **Entries**

- Readily identifiable focal point to signify tenant space(s)
- Increase height and bulk of appearance
- Wood beam or metal canopy trellis, stone columns, arch details
- Buildings facing onto Central Avenue shall have the entry on the cul-de-sac, except Parcel 1 which shall have entry from V Street and Parcel 4 which shall have entry from Barton Avenue, and both shall include architectural treatment and landscaping facing onto Central Avenue

### **Walls**

- Rectangular shapes emphasizing strength with two-story height and load-bearing appearance
- Compositions highlighted by wall projections and recesses to create vertical articulation
- Varied parapet heights with cornice molding detail
- Horizontal belt course/trim to define the first and second floor. The second story exterior is smooth with the first floor is articulated by texture, access color and patterns
- Regular patterns, with horizontal and vertical trim reveal coordinated with windows and door locations
- Stucco, masonry, or metal appearance
- Recessed and arcaded colonnades “loggias” at first floor

### **Roofs**

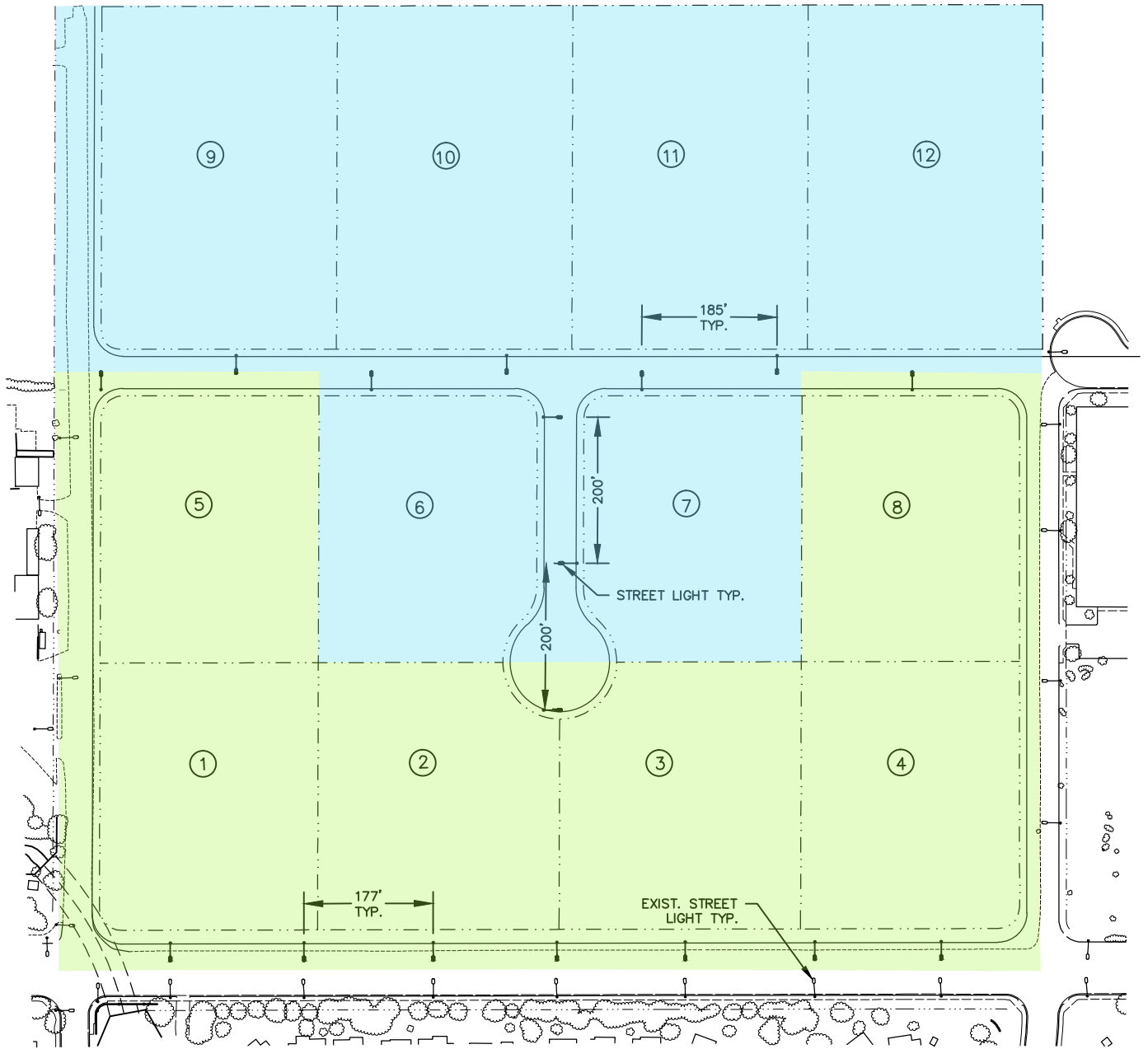
- Low slope/flat built-up roofs designed so as not to be visible from adjacent streets and shall be finished with a light-colored surface that is recommended for energy conservation
- Roofs designed to accommodate photovoltaic solar panels and skylights not visible from public right-of-way
- Low pitched, “S” tile or metal roofs with hip/pyramid shape and deep overhangs
- Boxed-in roof eaves with decorative brackets or exposed wood beam rafters

AREA  
B



AREA  
A





**LEGEND**

- Architectural Theme Area B
- Architectural Theme Area A



## **Glazing**

- Recessed within wall with trim or recessed surround
- Vertical proportioned windows align with openings above or below
- Consistent size and space at each floor
- Accent windows with arched top windows
- Exterior cantilevered shading devices, where applicable, to reduce glare and solar heat gain

## **3.2.2 Site Design Guidelines**

### **Access Driveways and Locations**

- Development standards shall be consistent with the Conceptual Circulation Plan.
- Development of common access drives should be utilized to minimize the number of curb cuts.
- Access drive designs should consider need for vehicle stacking during peak periods of use.
- No access is allowed along Central Avenue.
- V Street and Barton Avenue shall be widened at the intersections with Central Avenue for right turn truck movements.
- V Street shall be widened along the entire frontage of the eastern portion of the site.

### **Service and Loading Areas**

- On-site parking and loading facilities shall comply with the Lompoc Municipal Code, Section 10.32 and best practice design.
- Provisions should be made on each site for vehicle loading and on-street vehicle loading shall not be permitted.

### **Refuse Collection Areas**

- Outdoor refuse containers shall be visually screened within a durable six (6) foot or higher non-combustible enclosure, so as not to be visible from adjacent lots or sites, neighboring properties or streets.
- Tenants/Owners shall sign an agreement with the Solid Waste Division agreeing to move refuse containers to street on collection days.

- No refuse enclosure areas are permitted between a street and the front of a building.
- Refuse enclosures should be designed to contain all refuse generated on site and between collections, and should not be visible from outside the refuse container.
- Refuse collection enclosures should be designed of durable materials with colors which are complementary to building design.
- Refuse collection enclosures should be located to provide clear and convenient access to refuse collection vehicles.
- Refuse collection enclosures shall be designed and conveniently located for the deposition of refuse generated on site.
- Landscaping screening shall be located around refuse collection enclosures that are visible from public roadways.

#### **Screening of Exterior Mechanical and Electrical Equipment**

- Exterior components of plumbing, processing, heating, cooling and ventilation systems (including but not limited to piping, tanks, stacks, collectors, heating, cooling and ventilating equipment fans, blowers, ductwork, vents, louvers, meters, compressors, motors incinerators, ovens, etc.) shall not protrude above the top of a parapet.
- Building parapets shall be of sufficient height to screen visibility of roof mounted equipment so that screening devices are not required. If building parapet does not provide the required screening, mechanical equipment shall be fully screened as an integrated part of the overall architectural design.
- No exterior components of plumbing, processing, heating, cooling, and ventilating systems shall be mounted on front or side building walls unless they are an integrated architectural design feature.
- Transformers should be screened with a design configuration acceptable to the City of Lompoc, Electric Division.
- Transformer enclosures shall be designed of durable materials with finishes and colors complementary to the architectural theme.
- Electrical equipment shall be mounted on the interior of a building where possible. When interior mounting is not practical, electrical equipment shall be mounted in a location where it is substantially screened from public view. In no case shall exterior equipment be mounted on the street side or primary exposure side of any building.

- Exterior mounted electrical equipment and conduit shall be painted to blend with the architectural theme.

### **Fences and Walls**

- No fence or wall shall be constructed within the front setback except security, freestanding, or wing walls which are an approved part of the architectural design of the buildings.
- Fencing intended for security purposes and visible to the public shall be decorative metal channel, or other architecturally acceptable material and design, such as wrought iron, solid masonry, chain link, or barbed/razor wire may only be permitted with Planning Commission approval. Walls intended for site screening shall be decorative masonry or architecturally treated tilt-up concrete. Decorative masonry or tilt-up concrete walls shall be consistent with the architecture of the building.
- Fencing and walls shall be a maximum of eight (8) feet in height, unless allowed by a CUP granted by the Planning Commission.
- Screening and or obstruction of Fire Department connections will not be permitted.

### **Utilities and Communication Devices**

The placement and screening of backflow preventers, Fire Department connections, water meters, electric meters and cabinets, and other outdoor and connection devices should be considered early in the site design process. Backflow devices shall be screened from public view where possible by a low wall and/or vegetation, subject to allowing adequate Fire Department access and visual inspection. As backflow preventers must be located close to public main water lines, the location of water mains shall take in consideration of the visual impact of backflow preventers.

- On-site utilities including, but not limited to, drainage systems, sewers, gas lines, water lines, and electric (except transformers) telephone and communications wires and equipment shall be installed underground and maintained by the individual parcel owner.
- On-site underground utilities shall be designed and installed to minimize the disruption of off-site utilities, paving, and landscape during construction and maintenance and designed to not place excessive burdens upon off-site utility systems during the course of use.
- Temporary overhead power and telephone facilities are permitted only during construction.

### **3.3 ARCHITECTURAL STANDARDS**

Building materials and textures not permitted:

- Mirror glass windows
- Wood siding or trim on walls
- Wood or asphalt shingles, or wood shake roofs

Materials shall not be used in any high contrasting or graphic pattern that would cause visual distraction.

Building finishes and colors not permitted:

- Highly reflective finishes
- Bold colors with high contrast accents

## 4.0 CONCEPTUAL INFRASTRUCTURE AND PUBLIC SERVICES PLANS

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The development of the CCBPSP area will require the extension of existing infrastructure and services. The California Government Code requires a specific plan to include text and diagrams that specify “the proposed distribution, location and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the CCBPSP and needed to support the land uses described in the specific plan.” This section helps the CCBPSP fulfill this requirement and meets the Lompoc Zoning Code requirements for the content of specific plans. In addition, this section facilitates orderly development by identifying the utility infrastructure needed to support the permitted development.

### 4.1 WATER

Water supply to the CCBPSP area is currently supplied by an on-site water well. This well supplies water for agricultural irrigation. The existing well is located along the northern perimeter of the site and will be used during the course of construction. The well will be abandoned pursuant to state and local regulations, upon connection of the site to the City’s potable water line. Water supply during construction must comply with Section 3312 of the California Fire Code as amended and adopted by the City of Lompoc.

The City of Lompoc will provide water services to the site. The site is estimated to demand 39.05 acre-feet of water per year (afy), which is 0.11 acre-feet per day or approximately 34,867 gallons per day.<sup>1</sup>

**Exhibit 13, Conceptual Potable Water Plan**, shows the locations of potable water lines. Existing 10-inch water mains are located at the northeastern corner of the CCBPSP area and underneath Barton Avenue, V Street, and Central Avenue. Proposed 10-inch water mains would be located beneath Avila Street, Aviation Drive, V Street, and along the northern perimeter of the site and will connect to the existing 10-inch water mains at the northeast corner of the site and at Aviation Drive and V Street.

Landscape shall be designed and maintained to be water conserving and efficient, as specified in Lompoc Municipal Code Section 15.52, Water Efficient Landscape and Irrigation Standards, and Section 13.04.070, Development Project Impact on Water Supply. All proposals shall require a discretionary landscape plan approval.

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1 Indoor Demand = 15.64 afy; Outdoor Demand = 5.0 afy; Manufacturing Demand = 18.41 afy.  $15.64 + 5.0 + 18.41 = 39.05$  afy.

## 4.2 SANITARY SEWER DISPOSAL

The City of Lompoc will provide sanitary sewer services to the CCBPSP area. The contour grading concept in this CCBPSP has been designed to allow for all sewer flows to exit the site by gravity to the Lompoc Regional Wastewater Reclamation Plant. The on-site sanitary sewer pipes will be 6-inch and 10-inches in size. The system will be divided into two (2) areas, one (1) draining to the east and the other to the west and southwest. **Exhibit 14, Conceptual Sanitary Sewer System Plan**, shows the lines, directions, and points of connection.

The on-site sewer pipelines will exit the site to the east and connect to an existing 10-inch sewer line in West Barton Avenue and from there the existing line extends south to Central Avenue and west underneath Central Avenue. The on-site sewer pipelines will also exit the site to the west and then south underneath V Street to connect to an existing 12-inch sewer line underneath Central Avenue. Existing siphons underneath Central Avenue will be expanded to accommodate the additional sewer flows.

## 4.3 DRAINAGE

The CCBPSP area is fairly level and contains agricultural uses. Elevations will remain unchanged with minor variations to account of shrinkage of the soils and warping of drainage surfaces.

The CCBPSP area is fairly level. Existing State requirements stipulate that all drainage on new or reconstructed streets, new subdivisions, new development, and redevelopment must comply with both the City of Lompoc's Post-Construction Requirements and the City's Standard Requirements for the Design and Construction of Subdivisions and Special Developments, Section 5, Construction of Storm Drains. While a storm drain plan is provided in concept (see **Exhibit 15, Conceptual Storm Drain Plan**), the storm drains and curb drain inlets proposed are intended solely to address flood flows, as pre-development flows are to be infiltrated into the street right-of-way and on individual development sites. Trash interceptors and storm drain filters, as approved the City of Lompoc, will be required in each street inlet. Each parcel will be required to infiltrate the volume of water equal to 95 percent of the 85th percentile, 24-hour storm, as it falls over the impervious area developed on the site. Street improvements shall meet the same standard by incorporating Low-Impact Development (LID) features and infiltration areas within the right-of-way. Storm water runoff beyond the 85th percentile design storm will be transmitted through 18-inch, 24-inch, and 30-inch pipes along Aviation Drive, V Street, and the southern CCBPSP site boundary to discharge into the V Street Channel located at the southwest corner of V Street and Central Avenue. The existing storm drain inlet shall be removed and a new storm drain connection, without open public access, shall be installed.

**Exhibit 15** shows the locations of the proposed storm drains and curb inlets. The storm drain system will collect on-site runoff and direct it to on-site infiltration features. These features will have over flow outlets that will be transmit flood flows through 18-inch, 24-inch, and 30-inch pipes along Aviation Drive, V Street, and the southern site boundary to discharge into the channel located at the southwest corner of V Street and Central Avenue. All common area storm water control features, infrastructure and improvements will be maintained by the POA or Assessment District along all streets.

#### **4.4 ELECTRIC AND NATURAL GAS SERVICE**

The CCBPSP area is located within the service area of the City of Lompoc, Electric Division. The City maintains a large network of transmission and distribution infrastructure throughout the area in order to provide electrical power and service to its customers. The City will provide electric service to the CCBPSP area. New electrical lines will be placed underground. All conduits will be with full encasement.

The Southern California Gas Company (SoCal Gas) provides natural gas service throughout the Lompoc area. Most of the natural gas supply comes from out of the state. SoCal Gas will provide natural gas service to the CCBPSP area. Service connections will be provided and maintained throughout the CCBPSP area as needed.

#### **4.5 TELEPHONE & CABLE SERVICES**

Telephone service and maintenance to the area is provided by Verizon. Telephone facilities will be located underground within the public right-of-way. No overhead telephone facilities will be permitted.

Cable television is provided in the area by Comcast. Comcast will serve the CCBPSP area and cable facilities will be located underground within public right-of-way.

#### **4.6 SOLID WASTE DISPOSAL**

The City of Lompoc provides solid waste services to the City. The City provides weekly garbage and recycling collection services in the City and owns and operates the City of Lompoc Sanitary Landfill, which is a Class III (non-hazardous) landfill. Services provided include automated refuse, recycling, and green waste collection. Solid waste from commercial customers may be collected up to five (5) days per week (Monday through Friday), if required. Trash and green waste are hauled to the City of Lompoc Sanitary Landfill and recyclables are hauled to a Material Recovery Facility (MRF) located in Santa Maria.

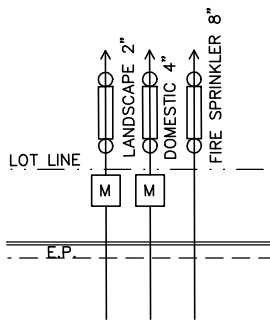
## **4.7 PUBLIC SERVICES**

### **4.7.1 Fire Protection Services**

A full range of fire protective services is provided by the Lompoc Fire Department. The Lompoc Fire Department operates two fire stations. Station No. 1, which is also the administrative headquarters, is located at 115 South G Street. Station No. 2 is located at 1100 North D Street and is the closest fire station to the CCBPSP area, located approximately one (1) mile southeast of the site. A fire station is proposed to the east of the CCBPSP area. Street improvements shall be coordinated with this proposed development to meet emergency accessibility requirements.

### **4.7.2 Law Enforcement Services**

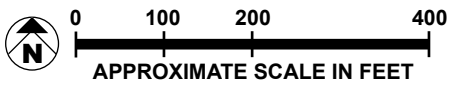
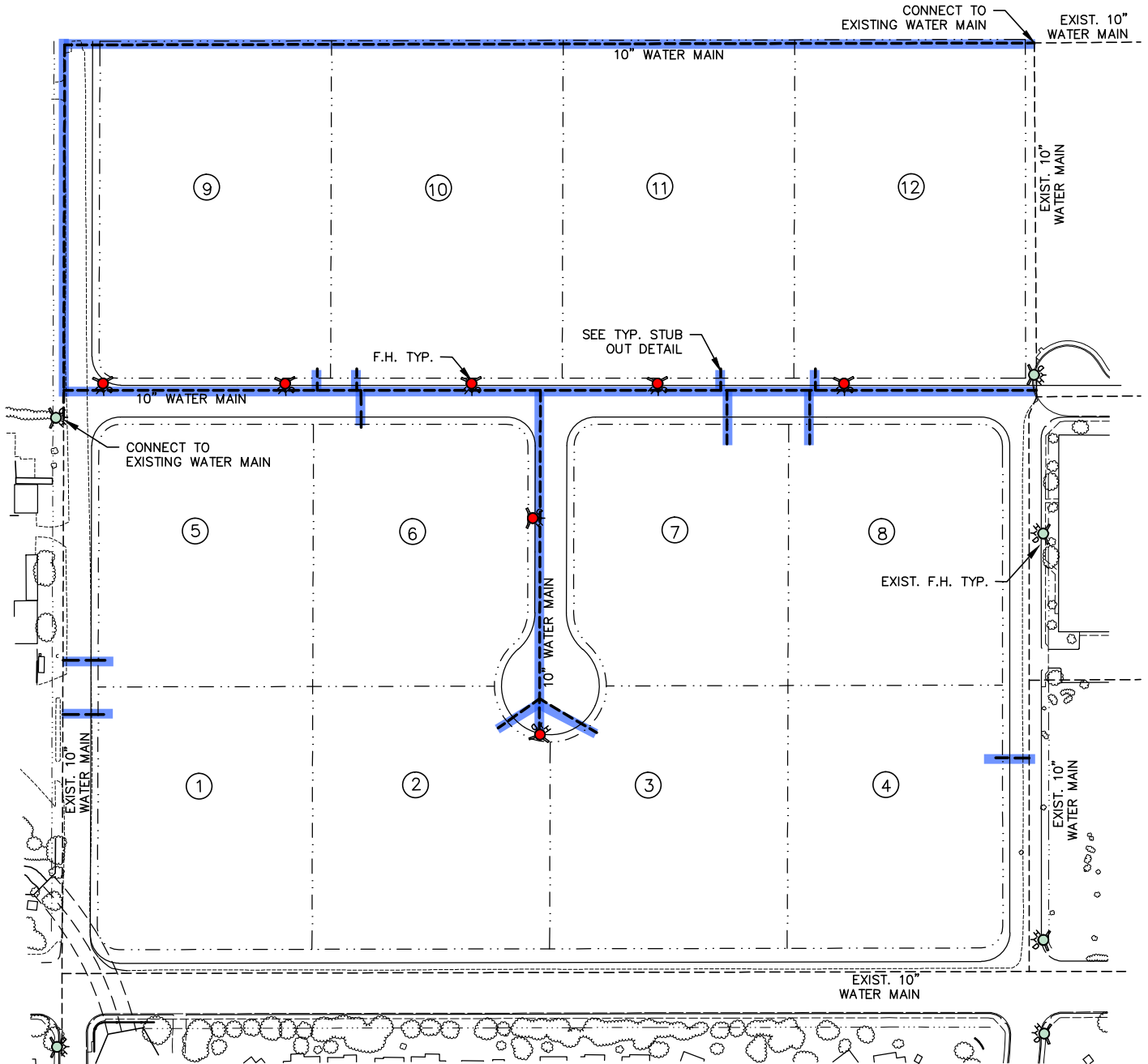
Law enforcement services are currently provided by the Lompoc Police Department, located at 107 Civic Center Plaza. The Lompoc Police Department is a full-service law enforcement agency and is located approximately two (2) miles southeast of the site.








**TYPICAL STUB OUTS  
AT EACH LOT**

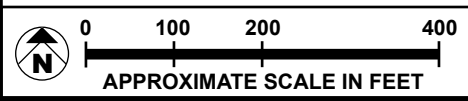
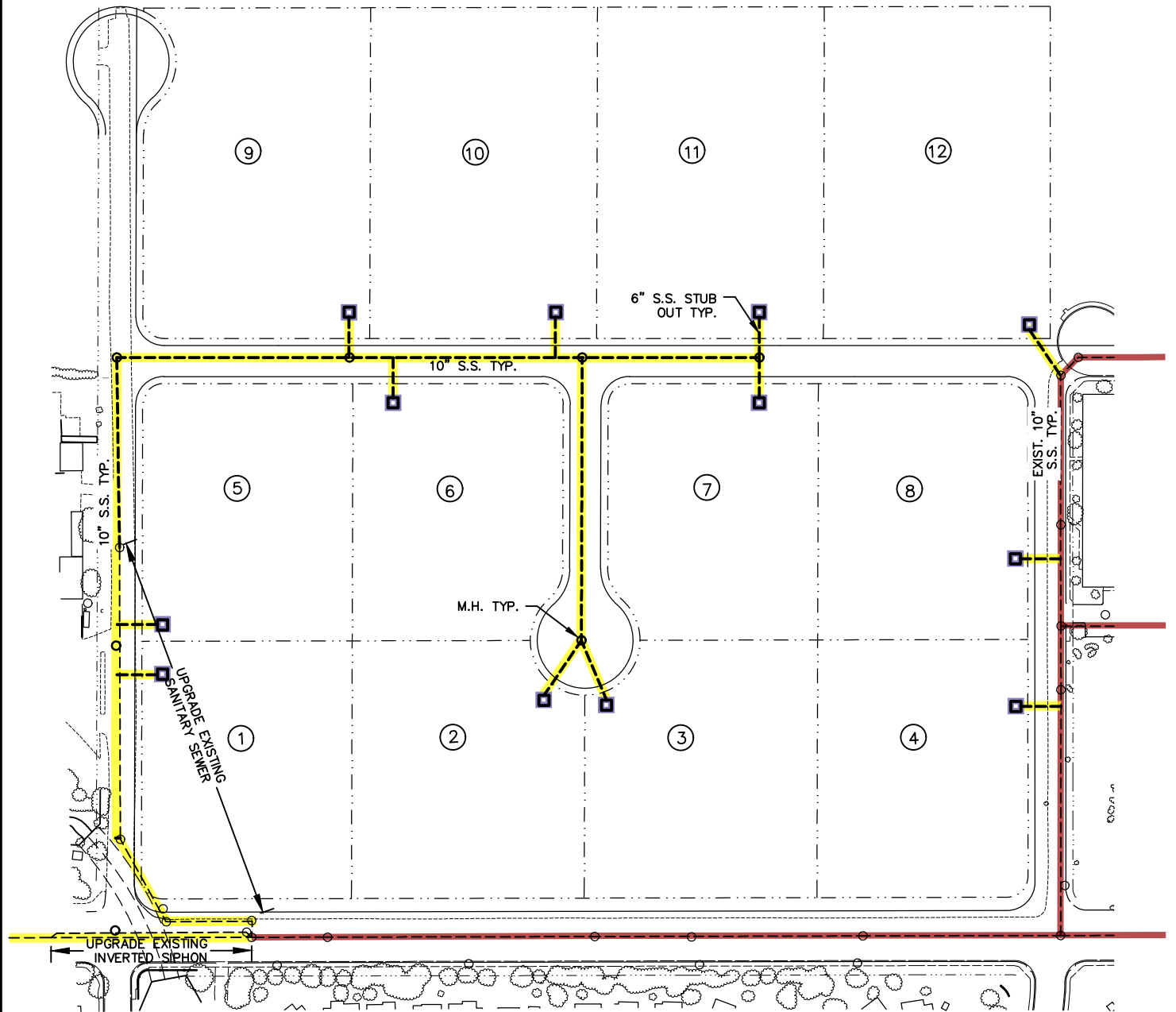
**LEGEND:**

- EXISTING WATER PIPE
- PROPOSED WATER PIPE
- ⊗ EXISTING FIRE HYDRANT
- ⊗ PROPOSED FIRE HYDRANT











**LEGEND:**

-  EXISTING SANITARY SEWER LINE
-  PROPOSED SANITARY SEWER LINE
-  EXISTING MANHOLE
-  PROPOSED MANHOLE
-  PROPOSED MONITORING VAULT



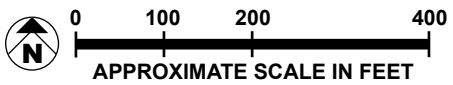
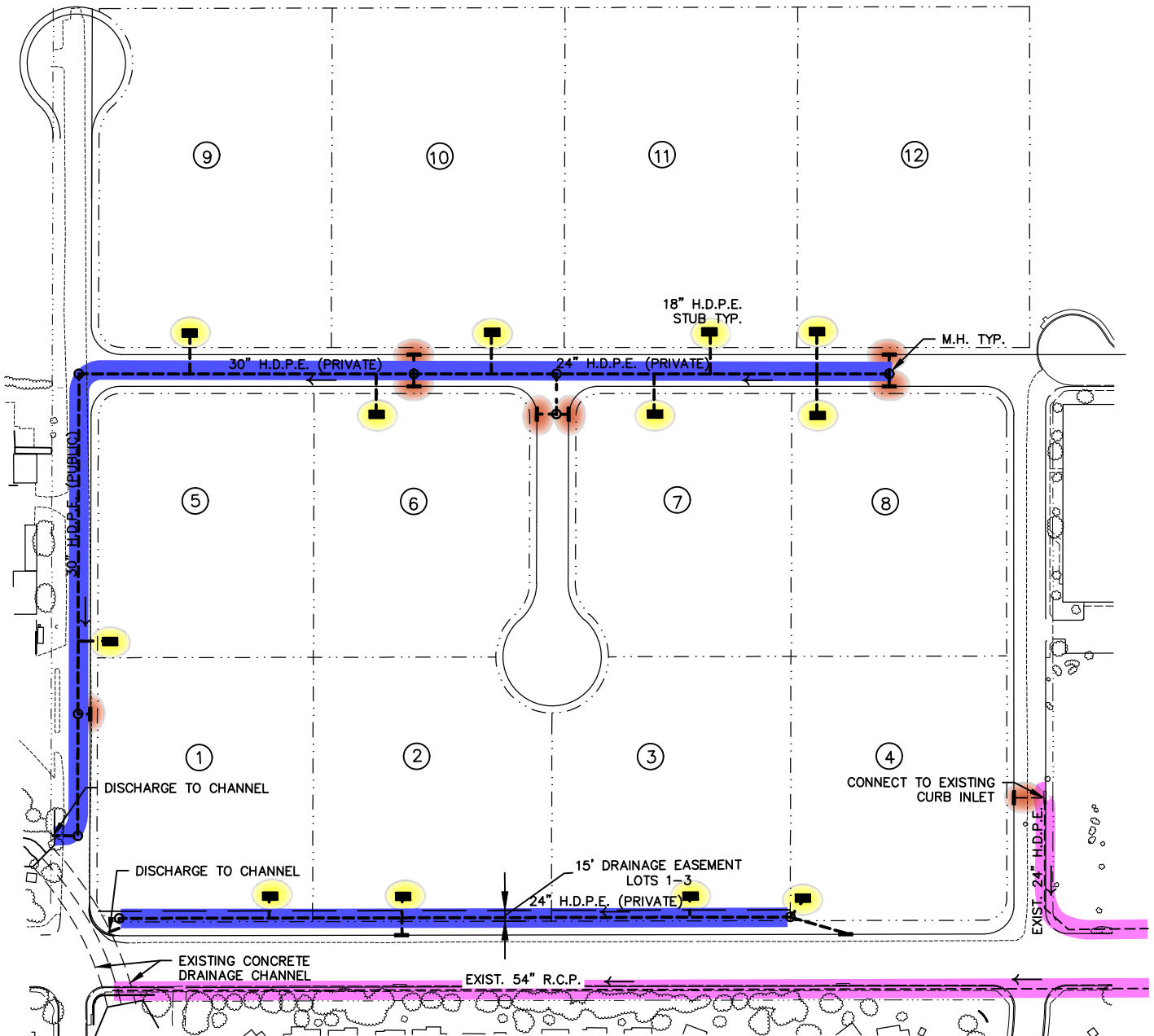
**LEGEND:**

-  EXISTING STORM DRAIN
-  PROPOSED STORM DRAIN
-  EXISTING CURB INLET
-  PROPOSED CURB INLET
-  EXISTING STORM DRAIN MANHOLE
-  PROPOSED STORM DRAIN MANHOLE
-  POINT OF DISCHARGE, INDIVIDUAL LOT
-  DIRECTION OF FLOW

INDIVIDUAL LOTS INCLUDE ON-SITE  
DETENTION/LID FEATURES TO RESTRICT  
FLOW TO MATCH PRE-DEVELOPED RATE.

STORM DRAIN INFRASTRUCTURE  
MAINTAINED BY POA.

ALL CHANNEL DISCHARGES SHALL BE  
APPROVED BY THE SANTA BARBARA  
COUNTY FLOOD CONTROL.



## **5.0 IMPLEMENTATION**

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### **5.1 PURPOSE AND INTENT**

Implementation procedures are intended to assure development in accordance with the design and development standards contained in this Specific Plan and other applicable City of Lompoc policies and regulations.

### **5.2 PUBLIC IMPROVEMENTS**

All public improvements and infrastructure will be completed by the CCBPSP developer.

#### **5.2.1 Property Owners' Association**

A Property Owners' Association (POA) shall be established to maintain common facilities, including but not limited to common areas, landscaping, and private streets. Street improvements, including the shared storm drain infrastructure, shall be maintained annually by the POA. A maintenance agreement with the City shall be required for landscaping along the site's frontage of Central Avenue, Barton Avenue, and V Street. A maintenance agreement to this effect shall be recorded with the deed for each lot.

### **5.3 ADMINISTRATION PROCEDURES**

The Central Coast Business Park Specific Plan and its provisions shall be implemented as the Zoning Regulations for the CCBPSP area. In the event of a conflict between the provisions of the CCBPSP and the provisions identified in the City of Lompoc Zoning Ordinance, the CCBPSP shall prevail. If the CCBPSP is silent regarding any development standard or process, the provisions identified in the City of Lompoc Zoning Ordinance shall prevail.

### **5.4 ADOPTION PROCEDURES**

In accordance with the California Government Code Sections 65453 to 65454, specific plans are required to be prepared, adopted, and amended in the same manner as general plans, except that specific plans may be adopted by resolution or by ordinance. This CCBPSP is adopted by resolution for General Plan consistency and by ordinance for zoning consistency.

### **5.5 SUBDIVISION PROCEDURES**

In accordance with the Subdivision Map Act of the State, the Land Surveyors Act, and Lompoc Municipal Code Chapter 16, no tentative or final subdivision map or parcel map shall be approved unless the

proposed land division, its design, and improvements are consistent with this Specific Plan, the Zoning Ordinance, and other plans and policies of the City.

## **5.6 AMENDMENT PROCEDURES**

Amendments of this CCBPSP shall be as often as deemed necessary by the City Council and shall be amended by resolution and ordinance of the City Council, as identified by Chapter 17 of the Lompoc Municipal Code.

## GENERAL PLAN CONSISTENCY

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California law requires a Specific Plan be consistent with the General Plan of the adopting locality. To ensure consistency with the General Plan, a review was done of the existing General Plan for relevant goals and policies. This review revealed the Specific Plan and the General Plan to be complementary and consistent. The following 2030 General Plan goals, policies, and implementation measures highlight this consistency with the Central Coast Business Park Specific Plan.

### LAND USE ELEMENT

The Specific Plan supports the General Plan Land Use Element by encouraging compact form development along Central Avenue adjacent to the Lompoc Airport and specifically by implementing and establishing standards for the Specific Plan. A variety of uses will be permitted within the Specific Plan area including manufacturing, warehouse, hangar/storage, and office space which will encourage future economic development adjacent to the Lompoc Airport.

Goal 1                      Maintain a compact urban form and growth pattern which provides adequate space to meet housing, employment, business, public health and public service needs.

Policy 1.3                      The City shall encourage development of under-developed and vacant land within its boundaries, and shall oppose urbanization of agricultural lands east of the City and west of Bailey Avenue.

Goal 3                      Encourage economic development by providing and maintaining opportunities for a diversity of commercial and industrial enterprises to meet the goods, services, and employment needs of Lompoc City and Valley residents, as well as to attain a balance of employment and housing within the Lompoc Valley.

Policy 3.1                      The City shall ensure that a sufficient and balanced supply of land continues to be available for residential, commercial, and industrial uses, with priority given to underdeveloped and vacant land within the City boundaries.

Policy 3.3                      The City shall protect existing commercially- and industrially-designated lands to ensure adequate space for non-residential development, to attract new business and employment centers, and to help achieve a jobs to housing balance in the City.

- Policy 3.8                    The City shall consider using a portion of the Transient Occupancy Tax to promote tourism and the visitor industry in Lompoc.
  
- Measure 13                The City shall amend the Zoning Ordinance to explicitly allow wine tasting rooms and winery-related facilities in appropriate commercial and industrial districts. [Policy 3.8]
  
- Goal 4                      Provide and maintain high-quality public facilities and services.
  
- Policy 4.4                 The City shall ensure that the impact of airport activities on sensitive land uses is minimized and that land uses in the vicinity of the Lompoc Airport are compatible with current and planned airport operations.
  
- Goal 5                      Protect the City’s and Lompoc Valley’s natural resources.
  
- Policy 5.4                 Development proposals in the vicinity of natural objects that have unique aesthetic significance shall not be permitted to block, alter, or degrade existing visual quality without the provision of suitable visual enhancement. This may include open space, eucalyptus groves, or vegetation that serves as a view corridor or has important visual attributes. Development proposals shall be sited to ensure that these features are retained or replaced to the extent feasible, resulting in minimal view impairment.
  
- Goal 6                      Protect the community against natural and man-made hazards.
  
- Policy 6.3                 The City shall require that all property owners located within an Airport Safety Area identified in the Santa Barbara County Airport Land Use Plan shall be notified, through property disclosure or other legal notice that runs with the land, that the property is within an officially designated Airport Safety Area.

**CIRCULATION ELEMENT**

Improvements to the pedestrian and vehicular circulation system are common to both the General Plan and this Specific Plan. This Specific Plan requires that Central Avenue, Barton Avenue, Aviation Drive, and V Street be constructed to full General Plan buildout, consistent with City standards. Bike lanes will be permitted consistent with the City’s Bikeway Master Plan.



Policy 1.9 The City shall ensure that developers of new commercial and mixed use areas provide adequate and convenient pedestrian access ways and bike ways into adjacent residential neighborhoods.

Policy 1.10 The City shall control access along expressways and arterials by limiting the number of intersections and driveways entering and exiting these high traffic roadways and by requiring that any development of new private driveways along such roadways does not introduce significant traffic conflicts.

Policy 1.11 To avoid the creation of new traffic flow hazards, the City SHALL require that future roads and improvements to existing roads be designed to minimize conflicting traffic movements such as overlapping use of turn lanes, curbside parking, and frequent stops.

Goal 2 Minimize the public's exposure to circulation-related noise and safety hazards.

Policy 2.1 The City's truck routes shall be designated along corridors that minimize traffic generated noise upon noise sensitive land uses (refer to Figure C-2).

Policy 2.2 The City shall encourage regulatory agencies to designate routes away from urban and environmentally-sensitive areas for transportation of hazardous and explosive materials.

Policy 2.3 The City shall ensure that approaches to intersection crosswalks and all adjacent street corners are illuminated by requiring all new commercial, entertainment, school and other pedestrian generating uses to provide lighting for pedestrians, for review and consideration by the City as part of the development review process.

Policy 2.5 As part of the development review process, the City shall require a system of sidewalks and multi-modal transportation pathways for all new development to provide a safe

environment for pedestrians and promote pedestrian and bicycle use.

Goal 3 Maximize the use and convenience of alternative transportation modes to reduce reliance on automobile use and reduce the associated vehicular traffic-related emissions.

Policy 3.1 The City shall provide and maintain a safe and convenient circulation system that encourages walking and that seeks to provide a continuous network of sidewalks and separated pedestrian paths connecting housing areas with major activity centers such as shopping areas, schools, and recreation.

Policy 3.2 The City shall provide and maintain a safe and convenient circulation system that encourages bicycle travel.

Policy 3.5 The City shall encourage regional transportation services to accommodate the needs of bicyclists, commuters and ridesharing.

Policy 3.12 The City shall require the inclusion of facilities that promote alternative modes of transportation, including marked bicycle lanes and connections, pedestrian and hiking trails, park and ride lots, and facilities for public transit into new development as well as existing development.

Goal 4 Protect and enhance the visual quality of Lompoc’s circulation system.

Policy 4.2 The City should maintain, and enhance where feasible, existing street trees, and shall encourage placement of new street trees in a manner that does not block pedestrian traffic or impair visibility of cross traffic in street frontage improvement projects.

Policy 4.5 Above ground utility cabinets shall be placed so they do not obstruct visibility of cross traffic.

Measure 9 The City shall limit on-street parking where feasible on certain roadways which are designated as bicycle routes in order to create new bicycle lanes and encourage bicycle travel. [Policies 1.1, 1.2 and 3.2]

- Measure 17 To accommodate the projected buildout traffic demands, Central Avenue shall be widened to its full planned width from “O” Street to “V” Street to allow for the required intersection improvements.
- Measure 24 Utility projects (communications, electrical, and water utility) shall be designed so that they do not obstruct driver’s visibility of cross traffic.

## **CONSERVATION AND OPEN SPACE**

The Lompoc General Plan goals and policies for resource management are supported by the Specific Plan’s archeological resource sensitivity, water supply management, and air quality project standards and guidelines.

- Goal 1 Protect native plant and animal habitats in recognition of their biological, educational, and scientific values.
- Policy 1.1 The City shall ensure that the biologically significant habitats, including but not limited to those within and adjacent to the Burton Mesa, the Santa Ynez River, San Miguelito Creek and Bailey Avenue Wetlands, are preserved, as defined in project-specific mitigation measures.
- Policy 1.2 The City shall encourage protection and mitigation of impacts to sensitive and critical habitats for special-status plant and animal species, as identified by the state and federal fish and wildlife agencies. [Final EIR Mitigation Measure BIO-2(a)]
- Policy 1.3 The City shall encourage the protection, preservation and restoration of native trees, particularly oak trees. [Final EIR Mitigation Measure BIO-2(b)]
- Policy 1.4 The City shall protect, as open space, the Santa Ynez River and its tributaries, which serve as flood channels, wildlife habitats, critical links in Lompoc's water supply, and components of the City's unique urban form. Watercourses shall be retained in a natural state where possible, rather than placed in concrete or placed underground.
- Policy 1.5 The City shall encourage the provision, maintenance, and protection of public access to publically-owned natural habitats and other open space areas in a manner that is not detrimental to their protection.



- Goal 5 Provide a sufficient supply of water to meet projected demand and minimize overdraft of the Lompoc groundwater basin.
- Policy 5.1 The City shall encourage water conservation, in order to efficiently utilize existing water supplies.
- Policy 5.2 The City shall continue to require new development to offset new water demand with savings from existing water users, and/or the in-lieu fee program to fund water conservation programs, such as recycled water for irrigation or percolation.
- Policy 5.3 The City shall continue to encourage the design and installation of energy conservation, water conservation, and solid waste reduction measures in all construction and rehabilitation projects.
- Goal 6 Protect and improve water quality in the Lompoc groundwater basin.
- Policy 6.1 The City shall review and condition development proposals to minimize adverse water quality impacts and contamination of the Lompoc groundwater basin.
- Policy 6.2 The City shall reduce waterborne pollutants and sedimentation from existing uses through implementation of the City’s Municipal Separate Storm Sewer Systems permit requirements, and its storm water ordinance and grading ordinance.
- Goal 7 Improve air quality in the Lompoc Valley
- Policy 7.3 The City shall rely on the Santa Barbara County Air Pollution Control District (SBCAPCD) to: 1) enforce air quality rules and regulations in a uniform manner and 2) maintain air quality monitoring stations in the Lompoc Valley.
- Policy 7.5 The City shall minimize air quality impacts resulting from construction and development activities regulated by the City by applying current recommended SBCAPCD conditions to development projects and implementing the City’s grading ordinance.

Policy 7.7	<p>Odor Abatement Plan. Future applicants proposing odor generating development shall develop and implement an Odor Abatement Plan (OAP). The OAP shall include the following:</p> <ul style="list-style-type: none"> <li>• Name and telephone number of contact person(s) responsible for logging and responding to odor complaints;</li> <li>• Policy and procedure describing the actions to be taken when an odor complaint is received, including the training provided to the responsible party on how to respond to an odor complaint;</li> <li>• Description of potential odor sources (i.e. odors associated with a fast food restaurant may include cooking and grease aromas);</li> <li>• Description of potential methods for reducing odors, including minimizing potential add-on air pollution control equipment; and</li> <li>• Contingency measures to curtail emissions in the event of a continuous public nuisance. [Final EIR Mitigation Measure AQ-3(a)]</li> </ul>
Goal 8	Minimize emissions from vehicles.
Policy 8.1	The City shall require development projects to minimize vehicle-related air quality impacts.
Goal 9	Reduce GHG emissions from municipal and community sources
Policy 9.1	The City shall participate in regional planning efforts with the SBCAG 2040 Regional Transportation Plan and the SBCAPCD to reduce basin-wide GHG emissions in compliance with AB 32 and SB 375.
Policy 9.2	New development subject to environmental review shall comply with California Environmental Quality Act guidelines for the analysis of greenhouse gas emissions developed pursuant to SB 97.
Policy 9.3	Through the CEQA environmental review process for discretionary permit applications, the City shall consider all feasible GHG emissions reduction measures to reduce direct and indirect emissions associated with project vehicle trip generation and energy consumption
Goal 10	Minimize per capita consumption of non-renewable energy resources within Lompoc.

- Policy 10.1            The City shall encourage community/neighborhood designs that minimize energy use.
- Policy 10.3            The City shall encourage site and building designs that minimize energy use.
- Policy 10.4            The City shall encourage the incorporation of feasible energy conservation measures into existing and new developments and structures. Feasible measures may include, but are not limited to, the incorporation of solar panels.
- Policy 11.5            The City shall work to improve non-motorized public access to designated open space areas in a way that protects environmental resources, but increases the ability of the public to enjoy and benefit from the open space.
- Measure 1             As part of the development review process, the City shall encourage avoidance of disturbance to environmentally-sensitive resources, including biologically-significant habitats. [Policies 1.1, 1.2, and 1.4]
- Measure 6             The City shall ensure landscape plans for projects adjacent to natural habitats incorporate the use of non-invasive local native vegetation compatible with the natural habitat. [Policy 1.5]
- Measure 8             The City shall use the Archaeological Sensitivity Zones Map, when updated, in conjunction with the City’s Cultural Resources Ordinance, to determine the appropriate level of cultural resource review for development projects. [Policy 2.3]
- Measure 19            The City shall require minimization of soil erosion, water quality degradation, and volume of surface water runoff during and after construction; and to maximize on-site percolation of stormwater. [Policy 5.7]
- Measure 23            The City shall ensure that new development implements Lompoc’s approved Post-development requirements. [Policy 6.3]
- Measure 25            The City shall continue pre-development coordination to ensure that applicants for new stationary sources of air pollution are notified of

APCD rules and regulations early in the project review process. [Policy 7.2]

Measure 27 Pursuant to Congestion Management Program goals, the City shall condition large commercial, industrial, and institutional developments to provide enhancements for: 1) users of alternative transportation modes; and 2) on-site services to reduce the need for offsite travel by employees. [Policy 9.1 and 9.2]

Measure 28 The City shall condition approval of individual development proposals on implementation of dust abatement measures. [Policy 8.5]

## **NOISE ELEMENT**

The Lompoc General Plan goals and policies to minimize noise problems are supported by the Specific Plan's design and guideline requirements for the Specific Plan area.

Goal 1 Minimize the amount of noise to which the community is currently exposed.

Policy 1.1 The City shall require each land use to maintain noise levels at their property line in compliance with City standards.

Policy 1.2 The City shall place a priority upon control of noise at the noise source.

Policy 1.8 The City shall encourage the use of alternative transportation modes such as bicycle paths and pedestrian walkways to minimize automobile traffic and its associated noise.

Goal 2 Minimize noise problems generated by future development.

Policy 2.1 The City shall use the noise standards presented in table entitled "Interior and Exterior Noise Standards" in determining land use designations and maximum noise levels allowable for new developments. In situations of overlapping Noise Standards, the lower noise level standard shall apply unless it can be found that the circumstances of the project allow for a less conservative interpretation based on the specific type of use, the benefits of the project, and the ability to mitigation the noise impacts.

- Policy 2.2                    The City shall require acoustical studies, prepared by a qualified acoustical engineer, for new development projects anticipated to either: (1) result in an ambient increase of five dBA Ldn; or (2) produce noise within five dBA/Ldn of the noise standard or greater than the noise standard for the proposed land use(s) under existing or future conditions. Should noise abatement be necessary, the City shall require the implementation of mitigation measures based on a detailed technical study prepared by a qualified acoustical engineer (i.e., a Registered Professional Engineer in the State of California with a minimum of three years of experience in acoustics).
- Policy 2.3                    The City shall minimize noise exposure in the vicinity of the Lompoc Airport by maintaining consistency with the adopted Lompoc Airport Master Plan.
- Policy 2.6                    External noise-generating equipment associated with commercial uses (e.g., HVAC units, etc.) that are located in mixed use developments and/or adjacent to residential uses shall be shielded or enclosed with solid sound barriers. [Final EIR Mitigation Measure N-3(c)]
- Policy 2.7                    Emphasize the following City preferred noise management strategies as an alternative to the construction of noise barriers:
- Avoid the placement of noise-sensitive uses within areas of high ambient noise
  - Orient buildings in a manner that shields noise sensitive portions of a project from noise sources
  - Use sound attenuating architectural design and building features

## **SAFETY ELEMENT**

The Safety Element of the General Plan does not conflict with the goals of the Specific Plan. The Specific Plan will require that new development within the Specific Plan area be consistent with the 2013 California Building Code and more specifically, not compound on-site or off-site flooding and minimize risks from seismic hazards.

- Goal 1                        Prevent injury, death, social, and economic disruption resulting from an extraordinary emergency.

Policy 1.1	The City shall strive to increase public awareness of emergency preparedness.
Policy 1.2	The City shall continue to improve responsiveness of City departments, during emergency situations, and encourage media, volunteer organizations, businesses, and the medical community to assist, as needed, during emergencies.
Policy 1.3	The City shall strive to ensure that critical facilities remain operational during and after a disaster (e.g. earthquake, flood).
Policy 1.4	The City shall avoid placement of critical facilities in hazardous areas, as identified on the hazard maps: <ul style="list-style-type: none"> <li>• Floodway or Floodway Fringe (Flood Hazard Areas Map);</li> <li>• Slope or Liquefaction Hazard Areas (Geologic and Soils Hazard Areas Map); and</li> <li>• High or Moderate Wildland Fire Areas (Wildland Fire Hazard Areas Map).</li> </ul>
Policy 1.5	The City shall provide adequate planning, organization and resources for emergency preparedness, access/evacuations and response.
Goal 2	Protect the community from loss of life and property resulting from flooding, while maintaining protection of natural resources located in flood hazard areas.
Policy 2.3	The City shall condition new development to ensure that it does not compound the potential for flooding.
Goal 4	Minimize risk to life and property associated with seismic activity.
Policy 4.3	The City shall ensure that all new development is constructed in accordance with current seismic safety design standards.
Goal 6	Minimize risk of fatalities resulting from radon gas exposure.
Policy 6.2	The City shall require new development and redevelopment to implement effective measures in construction to limit exposure to radon. [Equivalent to Final EIR Mitigation Measure GEO-5(a)]

- Goal 7 Protect the community through the safe and efficient production, use, storage, dispensing, use, handling, transport, and disposal of hazardous materials.
- Policy 7.1 The City shall encourage the safe and economical use, collection, storage, treatment, and disposal of hazardous materials generated by businesses and households.
- Policy 7.2 The City shall only permit development of facilities that produce, handle, store or transport hazardous materials in areas and in a manner that protects public health, safety, and the environment.
- Policy 7.3 To prevent hazardous material transportation incidents from affecting residential areas, the City shall, where feasible, create open space buffers between hazardous materials routes and residential neighborhoods.
- Policy 7.4 The use, storage, and handling of hazardous materials by businesses and industries in the City shall be conducted in compliance with all applicable federal, state and local regulations and guidelines.
- Policy 7.5 Residents within one quarter mile of hazardous materials handling facilities shall be notified immediately by the City of spills, leakages, or eruptions which may affect the health, safety and welfare of the public.
- Measure 16 The City’s development review process shall ensure the following: safe evacuation route(s); adequate peak load water supply; adequate minimum road widths Comprehensive Emergency Management and Recovery Plan, no less than two means of egress from planned unit developments, and adequate clearances around structures. [Policy 1.5, 3.2, 3.4, 3.9, 3.11]
- Measure 17 The City shall amend the Fire Protection Ordinance to allow the Fire Chief to require developments located in areas beyond the first due performance goal (six minutes 20 seconds from receipt of the call at the dispatch center, 90 percent of the time) to meet more stringent construction code requirements to provide necessary fire protection. [Policy 3.1]

- Measure 18                    The City shall amend the Subdivision Ordinance to establish maximum lengths of dead-end roads. The maximum lengths shall not exceed 350 feet for parcels containing less than 0.5 acre; 800 feet for parcels containing 0.5 acre to 0.9 acre; 1,320 feet for parcels containing 1.0 acre to 4.9 acres; and 2,940 feet for parcels containing 5.0 to 19.9 acres. All dead end roads will be provided with adequate turnarounds per Fire Department requirements. [Policy 3.1]
- Measure 26                    The City shall require the liquefaction potential to be evaluated by a Registered Soils Engineer for all critical facilities and major structures (reinforced concrete or steel-frame, two-stories or more in height) within the liquefaction hazard areas as shown on the Geologic & Soils Hazards map. [Policy 4.3]
- Measure 33                    At every potentially contaminated site proposed for development within the City, the project applicant shall have the site inspected by a qualified professional for the presence of hazardous materials and wastes. The City shall make certain that inspection reports are on file prior to project approval and prior to any excavation or construction. Acceptance of the site inspection report shall allow the proposed development to proceed to the permitting stage. All activities under this measure shall be performed in conformance with the policies and procedures presented in the Santa Barbara County Hazardous Waste Management Plan. [Policy 7.3]
- Measure 36                    For each specific project that would generate hazardous waste, the City shall require as a condition of building permit and/or business license approval that the project sponsor prepare a hazardous material handling program. The handling program shall identify the location of the new facility or use and designate either (1) specific routes to be used for transport of hazardous materials and wastes to and from the facility, or (2) specific routes to be avoided during transport of hazardous materials and wastes to and from the facility. Routes would be selected to minimize proximity to sensitive receptors to the greatest practical degree. Passage through residential neighborhoods shall be minimized, and parking of waste haulers on residential streets shall be prohibited. The City shall review and approve the applicant's hazardous

material handling program or, working with the applicant, modify it to the satisfaction of both parties. [Policy 7.2]

Measure 38 Open space buffers (landscape strips, masonry walls, etc.) shall be created between hazardous materials routes and residential neighborhoods. Also, residents within a quarter mile of new hazardous materials handling facilities shall be notified immediately by the City emergency response organizations of any accidental occurrences such as spills, leaks, or eruptions that may affect the health, safety, and welfare of the public. [Policy 7.3 and 7.5]

Measure 39 The City shall ensure that businesses and industries that use, store, and handle hazardous materials do so in compliance with applicable City policies as well as State and local laws, guidelines, and regulations. [Policy 7.2 and 7.4]

## **PARKS AND RECREATION ELEMENT**

The Lompoc General Plan goals and policies for parks and recreation are supported by the Specific Plan's landscape design and requirements along public roadways and within the Specific Plan Area.

Goal 1 Provide parkland and recreational facilities which are convenient to all neighborhoods and meet the needs of a diverse population.

Policy 1.5 The City shall encourage establishment of off-road bicycling/hiking/equestrian trails extending to Santa Ynez River Park, La Purisima Mission, State Burton Mesa Chaparral Preserve, Allan Hancock College, Ocean Beach Park, and along the Santa Ynez River.

Goal 3 All park and recreation facilities shall be well designed, developed, and maintained, as well as serve to enhance the positive aspects of the neighborhood.

Policy 3.1 The City shall encourage developments adjacent to parks or open space to provide direct access to, and common open space contiguous with, such areas.

Goal 4 The costs of providing parks and recreation facilities and programs shall be equitably shared by new development and current users.

Policy 4.1                      The City shall require all residential, commercial, and industrial developments to contribute toward acquisition and/or improvement of parks and recreation facilities.

## **PUBLIC SERVICES ELEMENT**

The Lompoc General Plan Public Services goals and policies are compatible with those of the Specific Plan.

Goal 1                      Provide safe, attractive, and efficiently designed facilities to serve public needs.

Policy 1.2                      The City shall ensure that all public buildings and facilities meet the access needs of physically-challenged individuals.

Policy 1.3                      The City shall collect development fees at a sufficient level to finance those public building and facility needs created and/or contributed by new development.

Goal 2                      Ensure a high level of public safety to the community.

Policy 2.6                      The Police Department shall review development projects for prevention of crime, vandalism, and traffic problems.

Goal 3                      Minimize loss of life, and property damage and reduce injuries due to fires, medical emergencies and other life threatening events.

Policy 3.7                      The Fire Department shall review all development projects for fire safety requirements.

Goal 4                      Provide high quality library facilities and services.

Policy 4.3                      The City shall cooperate with the Library Board of Trustees to ensure that improvements to the City of Lompoc library necessitated by new development within the City are proportionately financed by the project sponsor through development impact fees.

Goal 5                      Provide an economical and environmentally-safe solid waste collection and disposal system.

Policy 5.2	The City shall assure that sufficient capacity is available in the landfill prior to approval of new development projects.
Policy 5.3	The City shall assure that all improvements to the solid waste collection and disposal system necessitated by new development are proportionately financed by the project sponsor.
Goal 6	Maximize the life of the landfill.
Policy 6.1	The City shall continue to encourage efforts to reduce, recycle, and compost as many materials as possible.
Policy 6.2	The City shall strive to meet State goals to reduce waste entering the landfill.
Goal 8	Ensure the provision of adequate and high quality public educational facilities.
Policy 8.1	The City shall require payment of fees to mitigate impacts upon school facilities from new development within the City.
Goal 9	Provide economical and dependable water service.
Policy 9.2	The City shall assure that sufficient capacity and quality is available in the Lompoc Water Treatment Plant and system prior to approval of new development projects.
Policy 9.3	The City shall assure that all improvements to the water supply system necessitated by the approval of new projects are proportionately financed by the project sponsor.
Goal 10	Maximize the conservation of water.
Policy 10.1	The City shall promote the conservation of water by all customers.
Goal 11	Protect and improve water quality in the Lompoc Groundwater Basin.
Policy 11.1	The City shall continue to require public and private wastewater dischargers to connect to the City's waste water treatment system in order to minimize contamination of the Lompoc Groundwater Basin.

- Policy 11.2                    The City shall require that all new development be connected to a City-approved wastewater system.
- Policy 11.3                    The City shall incorporate water holding areas such as creekbeds, recessed athletic fields, ponds, cisterns, and other features that serve to recharge groundwater, reduce runoff, improve water quality and decrease flooding into the urban landscape.
- Policy 11.4                    The City shall identify and protect groundwater recharge areas to maintain suitable groundwater levels and to protect groundwater quality for existing and potential municipal water sources.
- Policy 11.5                    All land development shall include a Storm Water Pollution Prevention Plan in accordance with the federal Clean Water Act and current rules and procedures prescribed by the Regional Water Quality Control Board. Low Impact Development (LID) measures shall be incorporated into project design where feasible.
- Goal 12                        Provide economical and dependable sewer service and treatment.
- Policy 12.2                    The City shall assure that sufficient capacity is available in the Lompoc Regional Wastewater Reclamation Plant prior to approval of new development projects.
- Policy 12.3                    The City shall assure that all improvements to the sewer system necessitated by the approval of new projects are proportionately financed by the project sponsor.
- Goal 13                        Provide storm drains which minimize street flooding.
- Policy 13.2                    The City shall require new developments to: minimize the amount of off-site drainage by retaining stormwater for on-site percolation, provide adequate drainage facilities for remaining off-site flows, maintain natural drainage channels, and avoid alteration of off-site drainage courses.
- Goal 14                        Encourage development of technology infrastructure and service to allow Lompoc residents and businesses to efficiently communicate with individuals and institutions locally, regionally, nationally, and globally.

- Policy 14.3                    The City shall require new residential and commercial development to include infrastructure components necessary to support modern communication technologies.
- Goal 15                        Provide economical and dependable electrical service.
- Policy 15.2                    The City shall assure that the electrical system has sufficient available capacity prior to approval of new development projects.
- Policy 15.3                    The City shall assure that all improvements to the City electrical system necessitated by new development projects are proportionately financed by the project sponsor.
- Goal 16                        Provide attractive streets and neighborhoods by undergrounding new utility distribution lines.
- Policy 16.1                    The City shall require undergrounding of new utility distribution lines in association with new developments.
- Policy 16.2                    The City shall ensure that routes and facilities for pipelines and utility transmission lines are compatible with surrounding existing and planned land uses.
- Goal 17                        Maximize the conservation of electrical energy resources.
- Policy 17.1                    The City shall encourage the use of solar energy, in accordance /Public Utilities Code Section 2827, in the orientation and design of all new public and private development projects.
- Measure 6                     The City shall amend the City Code to require installation of automatic fire protection systems in all new buildings that exceed fire protection statutory square footage and on-scene response capabilities of the Fire Department. [Policy 3.5]
- Measure 10                    The City shall continue to collect Development Impact Fees to fund improvements to the City of Lompoc library system which are necessitated by new development. [Policy 4.3]

- Measure 23                    The City shall work with appropriate agencies to minimize water quality impacts from new development and other activities in the watersheds of the City’s water supplies. [Policy 9.2]
- Measure 24                    The City shall investigate and implement, if feasible, basin recharge programs through non-traditional methods. Such programs may include the following: storm drainage system design integrating Low-Impact Development (LID) features to reduce hydromodification from development and other improvements to recharge the ground water aquifer; developing/improving water recharge along historic drainage patterns along/adjacent to creeks and/or rivers; and/or developing recycled water irrigation programs including basin recharge. [Policy 9.1]
- Measure 25                    The City shall promote water conservation technologies such as low-flow showerheads and toilets, efficient clothes washers, irrigation controllers, and more efficient water-using industrial equipment should be incorporated in all new construction and retrofitted in remodeled buildings. [Policy 10.1]

## **URBAN DESIGN ELEMENT**

The Urban Design Element of the General Plan does not conflict with the goals of the Specific plan. Urban design recommendations from the General Plan include such policies to enhance Central Avenue and adjacent roadways and encourage a cohesive and high quality design and development within the Specific Plan Area. Urban design goals from the General Plan are also reflected in this Specific Plan include the following:

- Goal 4                            Protect and enhance the visual qualities of Lompoc's urban streetscapes and public places.
- Policy 4.1                      The City shall support efforts to improve the appearance of arterial roadway corridors.
- Policy 4.2                      The City shall promote cleanliness and regular maintenance of all neighborhoods and public places.
- Policy 4.3                      The City shall encourage signage to enhance the visual qualities of the urban streetscape.

Policy 4.4	The City shall continue to encourage provision of art in public places.
Policy 4.5	The City shall encourage the owners and/or operators of land uses and activities which are unsightly to clean up the affected area or to use landscaping and other design measures to soften or screen the area.
Policy 4.6	The City shall promote the development of the urban forest along streetscapes and in public places.
Goal 5	Ensure high-quality design and development.
Policy 5.1	The City shall ensure that all public and private improvements or development projects are consistent with the City's architectural, landscaping, and site design requirements.
Measure 12	The City shall establish a citywide street tree median and parkstrip planting program, if funding is available. Landscaped areas shall be located and designed to maintain vehicular and pedestrian safety, to beautify the roadways, and to maintain traffic-flow efficiency. [Policy 4.1]
Measure 13	The City shall explore the establishment of landscape maintenance districts, community facility districts, homeowner associations, particularly in new developments. [Policy 4.1]
Measure 19	<p>The City shall amend the Zoning Ordinance so the following glare reduction measures are applied when reviewing new development on a parcel specific basis:</p> <ul style="list-style-type: none"> <li>• Utilize trees or other forms of vegetation to screen and visually soften parking areas. This measure would reduce the amount of heat and glare generated from painted and chrome automobile surfaces and prevent expanses of stationary and moving automobiles;</li> <li>• Require use of hooded lights on focused-beam lamps for nighttime illumination in parking areas, shipping and receiving docks and within industrial developments. These lights direct the light beam towards the ground, which if a dark pavement, will not reflect light and cause spillage into neighboring areas; and</li> </ul>

- Require use of materials which reduce or diminish glare for windows in new developments. [Policy 5.1]

Measure 22

The City shall require that outdoor lighting around buildings, in parking lots, and along streets be placed and designed to prevent excessive overspill of lighting into residential areas. [Policy 5.1]

## **CONSISTENCY CONCLUSIONS**

No inconsistencies exist between the Lompoc General Plan Elements and the Central Coast Business Park Specific Plan. The General Plan provides a supportive foundation for the Specific Plan and reinforces its goals and policies for the area. The establishment of the Specific Plan, development criteria, design guidelines, urban design and landscape enhancement program within the Specific Plan area all combine to effectively implement the applicable objectives and policies established in the General Plan.

Adoption and implementation of the Specific Plan will help ensure that future development in the area is consistent with the overall vision of the area created by the General Plan and its individual goals and policies.